



West Hawthorn

URBAN DESIGN FRAMEWORK
OCTOBER 2006



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Executive Summary

Background

The West Hawthorn Urban Design Framework (WHUDF) has been produced by David Lock Associates in association with the Boroondara City Council, the WHUDF working group, Ratio Consultants Pty Ltd, Going Solar, Bryce Raworth Pty Ltd, 'Paroissien Grant and Associates Pty Ltd.

The WHUDF sets out an integrated design vision for the development of the study area for the next 10 – 15 years. It will establish key development principles and objectives focusing on issues such as the size, form and use of buildings, vehicle and pedestrian movement, environmentally sustainable development, residential amenity and the cultural and landscape assets of the area. The WHUDF shows how all of these components currently and potentially interrelate and how they integrate with the surrounding area to create an urban environment which is an asset to the community.

Study Area

The study area of this project focuses on the Burwood Road, Hawthorn corridor, commencing from Glenferrie Road through to St James Park.

Key External Influences

The following points are important to note with regard to the strategic context of the study area within metropolitan Melbourne:

- The study area is located approximately 5 km east of the Melbourne CBD, within a well-established inner-suburban fabric
- Burwood Road forms the spine of the study area
- The study area is bounded by the neighbouring commercial precincts at Glenferrie Road Hawthorn, Kew Junction, Bridge Road Richmond and Victoria Street Richmond
- The study area is strategically located within the metropolitan public transport network
- The study area is positioned between the Eastern and South-Eastern Freeways

Policy and Initiatives Background

The WHUDF integrates the conclusions of a number of other recent and current Council studies, policies and initiatives that have a direct impact on the study area.

Consultation

The West Hawthorn Urban Design Framework is an opportunity for Council and the community to work collaboratively to develop long-term vision for the West Hawthorn area.

Throughout the project, several opportunities have been offered to members of the community to obtain information and provide input into this project.

Vision

The document outlines a vision which is being pursued for the West Hawthorn area. It illustrates the intentions of Council and the community for the West Hawthorn area with respect to land use, built form, car parking, movement, the public realm and infrastructure. ESD principles have been considered in parallel to each of these categories separately to ensure no ESD issue is overlooked

Broad Strategies

The broad strategies proposed for West Hawthorn are defined under the following headings:

1. Land use and economics
2. Built form
3. Building design and management
4. Public realm
5. Movement
6. Infrastructure

The Future of West Hawthorn – The Precinct Plans

The study area has been broken up into 8 Precincts. The precinct plans provide recommendations for how West Hawthorn is expected to develop in the future.

For each precinct the following elements are discussed:

- Key Objectives
- Vision
- Preferred Outcomes

Overall Strategy/ Concept

The overall strategy/ concept is a plan of the study area that identifies recommendations for changes to, and management of, the public realm.

1.0 Background

The West Hawthorn Urban Design Framework (WHUDF) has been produced by David Lock Associates in association with the Boroondara City Council, the WHUDF working group, Ratio Consultants Pty Ltd, Going Solar, Bryce Raworth Pty Ltd, 'Paroissien Grant and Associates Pty Ltd.

During the last 15 years ongoing development pressure in the area has resulted in developments that challenged the existing planning guidelines and the communities view on appropriate development. In order to work with the community to develop an overall strategic plan Council instigated the development of the WHUDF.

The WHUDF was triggered by a number of factors, including:

- a surge of recent development applications along Burwood Road which have attracted community concern
- community interest in the form of development along the interface with residential areas, particularly that interface with houses fronting Manningtree Road
- to preserve the large scale sites along Burwood Road which are unique in size to the City of Boroondara. With the decline of manufacturing in Burwood Road, these sites are important in providing opportunities for replacement industries such as large format retail space (bulky goods) and large format office space

Burwood Road provides a relatively unique opportunity within Boroondara for larger retail, office and apartment developments close to the Principal Public Transport Network. However, such development must be managed to ensure that it does not adversely impact on the character and amenity of the area and in particular, the surrounding residential neighbourhoods.

A clear direction is therefore required in relation to the future role of the project area. In addition, the UDF is to determine the preferred built form character—including the definition of boundaries and identification of opportunities for change, renewal and diversification—determine improvements required to public infrastructure, and identify the mechanisms necessary to achieve these outcomes.

The UDF will provide an overall strategy to guide development with specific recommendations in relation to key opportunities for development and improvements to the public realm.

An Urban Design Framework Background Report was prepared to assist in the production of the draft UDF. The Background Report identifies existing conditions, notes technical advice received, and provides rationale behind recommendations and strategies identified in the UDF. The Background Report is publicly available, but does not form part of the UDF.

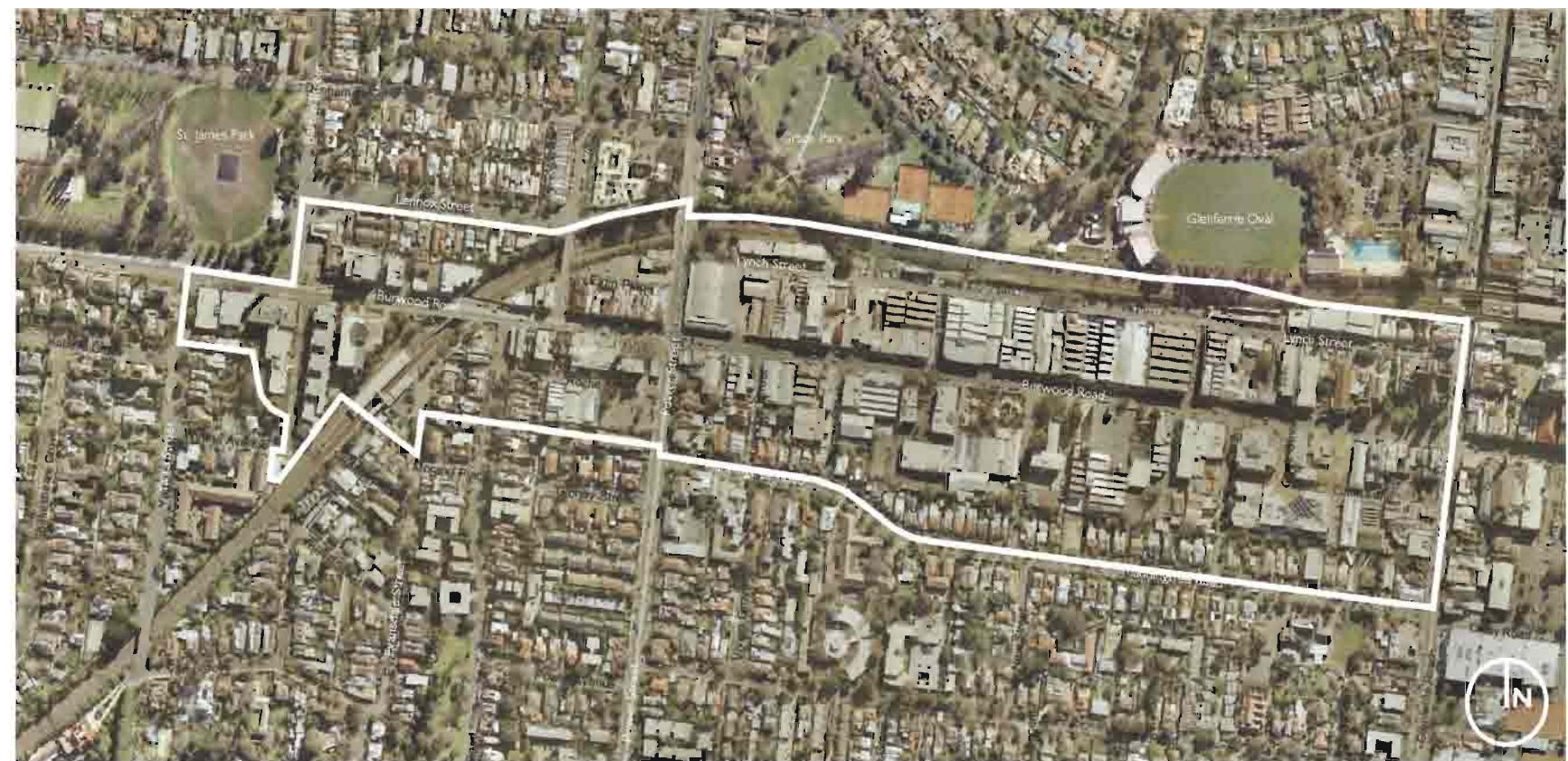
1.1 The Study Area

The study area of this project focuses on the Burwood Road, Hawthorn corridor, commencing from Glenferrie Road through to St James Park. (See Figure 1 below)

The specific study area is defined by the following features:

- to the north by the Burwood Road, Lennox Street, and the Belgrave Lilydale railway corridor
- to the south by Manningtree Road and the boundary between neighbouring residential properties
- to the west by Yarra Street, Barton Street, a drainage channel and the boundary between neighbouring residential properties
- to the east by Glenferrie Road

The study area is predominately located within the business zone with a surrounding residential interface.



 Precinct Boundary

Figure 1 The Study Area

1.2 Key External Influences

The following points are important to note with regard to the strategic context of the study area within metropolitan Melbourne:

- The study area is located approximately 5 km east of the Melbourne CBD within a well-established inner-suburban fabric
- Burwood Road forms the spine of the study area and is a section of a major arterial and commercial route which extends east from the Melbourne CBD and includes Flinders Street, Wellington Parade and Bridge Road
- The study area is bounded by the neighbouring commercial precincts at Glenferrie Road Hawthorn, Kew Junction, Bridge Road Richmond and Victoria Street Richmond
- The study area is strategically located within the metropolitan public transport network. The Lilydale/ Belgrave Rail Line runs through the study area, parallel with Burwood Road. Hawthorn Station is located within the study area and Glenferrie Station is located immediately to the east. Trams 109 and 75 also service the study area providing linkages to Port Melbourne and the CBD
- The study area is also strategically located for vehicle access. It is positioned between the Eastern and South-Eastern Freeways, whilst Burwood and Glenferrie Roads are also important arterial routes within the metropolitan road network

1.3 Policy and Initiatives Background

This WHUDF integrates the conclusions of a number of other recent and current Council studies, policies and initiatives that have a direct impact on the study area. These deal with a broad range of initiatives including ESD, greenhouse strategies, walking, neighbourhood development, building design, integrated transport, sustainable transport, community development, statutory development and safety and security.

Some of the policies considered during the creation of the UDF include:

- Disability Discrimination Act 1992 (Federal)
- Melbourne 2030 (State)
- Linking Melbourne: Metropolitan Transport Plan (State)
- Growing Victoria Together (State)
- The Sustainable State (TSS) (State)
- The Victorian Greenhouse Strategy (State)
- Environmentally Sustainable Design and Construction: Principles and Guidelines for Capital Works Projects 2003 (State)

- Guidelines for Higher Density Residential Development, 2004 (State)
- BEST – Assessing Ecological Impact (State)
- Environmental Management Systems in Government (State)
- Safer Design Guidelines for Victoria 2005 (State)
- State Planning Policy Framework (SPPF)
- Boroondara Municipal Strategic Statement (MSS) (Local)
- Walking School Bus (Local)
- Boroondara Bicycle Users' Group (Local)
- Boroondara Municipal Health Plan (Local)
- Heritage Policy (Local)
- Office Use & Development Policy (Local)
- Retail Centre Policy (Local)



A selection of images taken from within West Hawthorn

2.0 Consultation

The West Hawthorn Urban Design Framework is an opportunity for Council and the community to work collaboratively to develop a long-term vision for the West Hawthorn area.

Council established a community based Working Group that included: residents; trader representatives; commercial landowners; Councillors; and Council Officers to assist in the development of the WHUDF. The role of the Working Group was to develop a vision for the study area as representatives of key interest groups in the community.

Throughout the project several opportunities have been offered to members of the community to obtain information and provide input into this project. These include:

- The opportunity to provide views to Working Group representatives
- Attendance at Working Group meetings which included public question time
- Project updates and notes from Working Group meetings being made available on Council's website
- Formal consultation periods where letters and information have been sent to the West Hawthorn area

The development of the WHUDF has occurred in three stages. See Figure 2

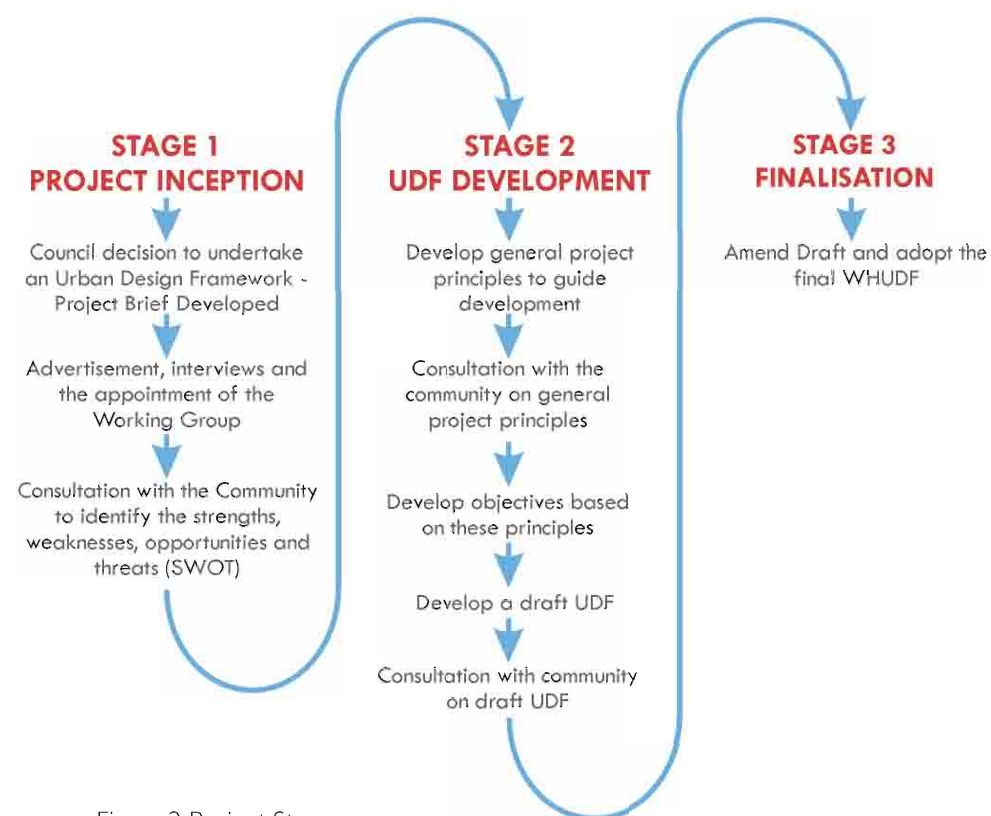


Figure 2 Project Stages

At the commencement of the project, the West Hawthorn community were invited to attend a community SWOT session where Strengths, Weaknesses, Opportunities and Threats were identified for the West Hawthorn area. This session provided the foundation for the West Hawthorn UDF Working Group to commence its task.

The views of the community were later sought to analyse 116 very detailed preferred outcomes for the study area developed by the Working Group. This feedback was analysed and assisted in refining preferred outcomes and the development of 20 principles which each underpin the draft WHUDF. The final stage of consultation allowed the community the opportunity to comment on the Draft UDF.

3.0 The Vision

This chapter is intended to describe the broader vision which is being pursued for the West Hawthorn area. The following information illustrates the intentions of Council and the community for the West Hawthorn area with respect to land use, built form, car parking, movement, the public realm and infrastructure. ESD principles have been considered in parallel to each of these categories separately to ensure no ESD issue is overlooked.

The defining of a vision helps to establish the objectives of the UDF, by clearly identifying the gap between what exists now and what could be. The vision also provides a benchmark against which specific proposals should be assessed. Only proposals that conform with and help realise the vision should be supported in the future.

Developing a UDF allows Council and the community to outline their vision as to how an area should be developed. Developers are then required to adhere to this vision and Council will refer to the UDF when assessing future development proposals.

3.1 A Vision for West Hawthorn

A vision for how West Hawthorn could be in future has been developed considering state and local government policy and the principles and objectives developed for the WHUDF. It has 7 components.

- West Hawthorn is a place that provides a diverse range of housing supporting community interaction, safety and amenity for residents and visitors alike
- West Hawthorn is a place that has increased its strategic importance as a location for bulky goods, homewares uses and offices whilst providing a range of other employment opportunities in retail, commercial and community based land uses
- West Hawthorn is a place that respects and builds upon its local character and heritage

- West Hawthorn is a place that can accommodate change in built form and landscape in strategic locations
- West Hawthorn is well connected to other places. It is able to be reached by many sustainable modes of transport in comfort and in good time
- West Hawthorn is a place that is ecologically sustainable.
- West Hawthorn is a place that is well regarded for high quality built form and landscape

3.2 The WHUDF Principles

The WHUDF is a way of expressing the vision for West Hawthorn. The community and Council have undergone an extensive period of consultation and collaboration to establish the following key principles that together start to outline a vision for West Hawthorn.

The principles as categorized under specific headings are as follows:-

ACCESS AND MOVEMENT:

PRINCIPLE:

1. Provide access and inclusion to West Hawthorn for people of all abilities
2. Create a high quality pedestrian environment
3. Reduce reliance on private car usage
4. Improve the management of car parking
5. Facilitate greater efficiency, access and safety of the public transport system

ECOLOGICAL SUSTAINABLE DEVELOPMENT (ESD)

PRINCIPLE:

6. Ensure ecologically sustainable design, construction and practices
7. Improve water use, reuse and on site management
8. Ensure adequate sunlight to new and existing development

and pedestrian routes

NEIGHBOURHOOD CHARACTER & HERITAGE:

PRINCIPLE:

9. Protect and enhance heritage places within the Study Area
10. Ensure that new development respects the siting, scale, and amenity of all valued and appropriate existing development
11. Ensure development responds to key view corridors and topography
12. Ensure all service, plant and infrastructure structures on buildings are not readily visible
13. Establish a definitive character for the Study Area through landscape treatments

URBAN DESIGN & LANDSCAPE:

PRINCIPLE:

14. Improve the public realm to promote the health and wellbeing of the community
15. Strengthen landscaping treatments to improve legibility and amenity
16. Improve pedestrian comfort and weather protection along pedestrian routes and in public spaces
17. Encourage landscaping that positively contributes to the character of the area
18. Use landscaping to soften the visual impacts of built form, roads and off street car parking areas

INFRASTRUCTURE & SERVICES

PRINCIPLE:

19. Provide community services/facilities that accommodate current and future needs of the West Hawthorn community
20. Ensure that new development provides for infrastructure to meet its needs and does not overburden the infrastructure capacity of the Study Area

4.0 Strategies

West Hawthorn is a mature, well-established inner suburb, and is located in close proximity to the major shopping centres of Glenferrie Road Hawthorn, Kew Junction and Bridge Road Richmond. St James Park, Glenferrie Road, Manningtree Road and the railway line comprise the boundaries of the study area.

Public transport provision is particularly good with tram and train services to the area. Vehicular access is also relatively good with access available from both the Eastern and South-Eastern freeways. The mixed use nature of the study area is demonstrated by the visitation and demographic characteristics of the catchment. The key characteristics to note are:

- The visitation patterns to the study area indicate a large proportion of people visit frequently, for work or shopping.
- Office use is spread evenly throughout the study area while the West Hawthorn Village plays a neighbourhood shopping centre role.
- A high number of university-educated, high income professionals can be found in the study area.
- Visitation is primarily by car, but up to 22% of trips are by public transport reflecting the relatively good access to public transport in the immediate area. The office and retail uses have a similar frequency of visitation, but are discernable due to the variation in time spent in the centre. As such they have different travel patterns.
- New residential developments have further consolidated the area as a mixed-use district.

4.1 A Sustainable West Hawthorn

“Sustainability” can be defined as “meeting the needs of the present without compromising the ability of future generations to meet their own needs.” Brundtland Commission 1987

A key focus of the UDF is to make West Hawthorn a sustainable place. A key to delivering the future sustainability of West Hawthorn is managing and retaining a high level of residential amenity whilst identifying opportunity for intensification of the built form, activities and employment to capitalise on the study area's proximity to public transport, existing services and existing commercial strength in line with the principles of Melbourne 2030.

4.2 A Liveable West Hawthorn

The UDF provides guidance on appropriate building forms within the Study Area. Building heights are influenced by a number of factors. Typically, these may include market demand, urban character, solar access, services and infrastructure capacity. The key built form issue confronting the study area is whether higher built forms are appropriate, and if so, where and how high. Reasons for considering higher built form and increased densities in the study area include:

- To address State planning policy (Melbourne 2030);
- To direct development pressure away from sensitive residential areas, heritage areas and areas with highly valued built form character.
- To minimise private car usage and urban sprawl;
- To capitalise on existing infrastructure;
- To increase housing diversity;
- To increase the viability of local services and business;
- To increase local jobs;
- To promote reinvestment;
- To enhance perceived safety, through increased activity; and
- To maximise building energy efficiency.

There are reasons to be careful about the height and location of taller buildings. Increased building height can have potential impacts on:

- Existing heritage and local character;
- Important views;
- Public and private amenity;
- Local traffic and parking;
- Services capacity; and
- Overall built form appearance.

There are a number of characteristics of an area that allow for higher built form to be accommodated. These are as follows:

- Areas where the amenity impacts of higher built form on an adjoining residential property can be appropriately managed.

- Areas where existing built form character is not as valued as other areas.
- Areas where services and facilities can be accessed by foot (ie. 400m). This reduces the need for car travel and maximises accessibility to services.
- Areas that are accessible to public transport to encourage public transport use and reduce car dependence.
- Areas where there are larger lots on which larger scale redevelopment may be more feasible and therefore most likely to eventuate.
- Areas where development will not impact negatively on views and vistas to landmark buildings.

4.3 Expected levels of change

The Study Area has been divided into eight precincts to recognise the different characteristics and context of each precinct. On this basis, the appropriate level of change has been identified using the following classifications: minimal; incremental; and substantial level of change. These terms are used to define the rate and nature of change envisaged to occur in each precinct as follows:

- Substantial Change - change that does not conform with the existing character but instead conforms with a preferred future character.
- Incremental Change - change that contributes to a gradual evolution of the existing character over time.
- Minimal Change - change that conforms with the existing character.

In Section 5 and 6 of this report, the rate of change has been translated into a built form scenario ie. low built form and medium built form. Low built form refers to buildings up to 2 storeys in height. Medium built form refers to buildings up to 5 storeys in height. These building heights are indicative only. They have been defined assuming a 4.0 metre floor to ceiling height at the ground level and 3.0 metre floor to ceiling height at the upper levels.

It should be noted that other floor to ceiling heights may be used however the standards identified within this framework are to be adhered to regardless of the number of floors developed.

4.3.1 Substantial Change

Precincts 1, 4A and 4B are identified as being most suitable for medium built form. This is due to their proximity to the Glenferrie Road and West Hawthorn shopping centres, railway stations and tram routes, their predominance of wider lots and their lack of valued built form character. However development will need to step down to the heritage places and be sensitive to the residential interfaces. Development in Precinct 4A may step up substantially at Lynch Street, given the lack of potential for tall development on the northern side of that street avoiding any risk of a 'canyon effect', and its lack of importance as a pedestrian route. It is also acknowledged that there is little likelihood of significant change in Precinct 1 in the foreseeable future, given the relatively young age of the buildings. The spire of the Immaculate Conception Church on the corner of Glenferrie and Burwood Road's should remain a dominant skyline feature and should not be silhouetted or obscured by future high-rise developments.

4.3.2 Incremental Change

There are some parts of Precincts 3 and 6 which can accommodate change allowing for medium built form. The ability to accommodate incremental change is due to their proximity to railway stations and activity centres, the predominance of wider lots and lack of consistent, valued built form character in some parts. There are some parts of these precincts that due to their proximity to heritage places and residential properties will only accommodate low built form.

The non-VicTrack land in Precinct 5 is suitable for low built form (at present the majority of the land is undeveloped) due to the small size of its lots and despite its proximity to the stations and activity centres.

4.3.3 Minimal Change

Precincts 7 and 2 are considered to be constrained by their heritage residential character and subsequently only have scope for low built form.

4.4 Building Width

In areas with a highly-valued building character, the typical building width often contributes significantly to this character. In particular, the West Hawthorn Shopping Centre and Glenferrie Activity Centres have predominantly narrow lots. The predominance of narrow lots helps to create the diverse range of shops and services and built form that is central to the area's character. The replacement of more than two of these traditional narrow buildings with a single, more substantial building would therefore be detrimental to the character of these areas.

The replacement of more than two traditional narrow buildings with a single wider building is strongly discouraged in the West Hawthorn Shopping Centre and Glenferrie Activity Centre.

4.5 Setbacks

Standards exist in the planning scheme (Clause 55), and in the form of DSE guidelines (Guidelines for Higher Density Residential Development - DSE 2004) to ensure that new residential developments avoid unreasonable impacts on other residential properties. It is considered appropriate that these standards should also apply to any development in the Study Area abutting a residential property in order to preserve residential amenity. Special provision has been made within the UDF to guide the form of development in special circumstances such as along Burwood Road. These special circumstances and the controls that apply to them are discussed below.

4.5.1 Front setbacks

The existing buildings along Burwood Road, Glenferrie Road and Power Street are predominately built to the front boundary producing a built form character that expresses these as commercial precincts. This character should be repeated and where possible new buildings should also be built to the front boundary so as not to affect the existing character and amenity of these roads. Therefore, it is proposed that new development on Burwood Road be built to the front boundary.

The height of buildings on street frontages can impact the feeling of openness for people on the street. The height of buildings along the northern side of a street determines the amount of sunlight that falls on the street and footpath including in the space between

the buildings. The amount of sunlight that falls on the street is affected by the time of year. The shadow of a building cast in a street increases in size during the winter months leading up to the winter solstice (21st June) and lessens in the summer months until the shadow is at its shortest at the summer solstice (22nd January).

Consideration of openness and sunlight to the footpath (along the south side of Burwood Road) has resulted in the following preferred outcomes being applied in precincts 1, 3, 4A, 4B and 6:

Buildings are to be of a height and setback such that they maintain street width to building height ratio of 1:0.5 at the front boundary line. Subsequent built form above 10.0 metres – behind the front boundary line – is to be stepped back so that it is out of sight when viewed from a height of 1.7 metres at the front boundary line on the opposite side of the street.

Where the building character is highly valued - such as in West Hawthorn Village - it is considered that new development above existing shopfronts should be set back so that it cannot be seen above the apparent street wall height when viewed from the opposite side of the street at 1.7 metres above the ground (average standing eye level). This is in order to avoid detracting from the predominant character.

See Figures 3 and 4.

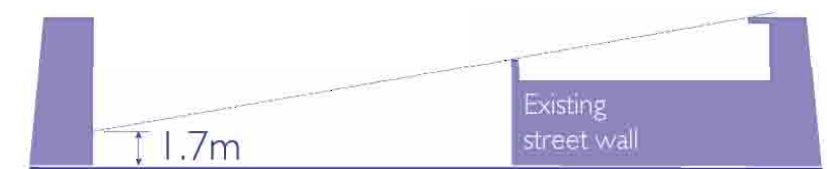


Figure 3 Example of a front setback for shoptop development that could be applied in Hawthorn Village



Figure 4 Example of a front setback for shoptop development that could be applied in Hawthorn Village

4.5.2 Side Setbacks

The existing buildings along Burwood Road almost all extend across the full width of the front boundary, creating a consistent character and well-defined public realm that avoids concealed spaces and a consequent lack of perceived safety. It is therefore proposed that all new development in Burwood Road be similarly required to extend across the full width of the lot at least at ground floor level except where access requirements do not allow this.

4.5.3 Rear Setbacks

Some properties fronting Manningtree Road and Lynch Street have an interface (common boundary) with large lots fronting Burwood Road. Several large lots fronting Burwood Road can potentially accommodate buildings of significant proportions. To minimise the potential bulk and visual impact of such buildings, measures are specified in the UDF. See Figures 5 and 6.

- Development to be of a height that would be obscured by a building that is 7.0 metres in height and is setback 5.0 metres from the property boundary when viewed from the back of dwellings that front Manningtree Road and Lynch Street. The development is to be viewed from an eye height of 1.7 metres and at a distance of 13.0 metres from the boundary.
- Development to be setback no less than 5.0 metres from the boundary.
- Introduce a significant landscape buffer of no less than 5.0 metres in width between the boundary and building. Note that the building may be set back further to allow for a staff carparking and service area.

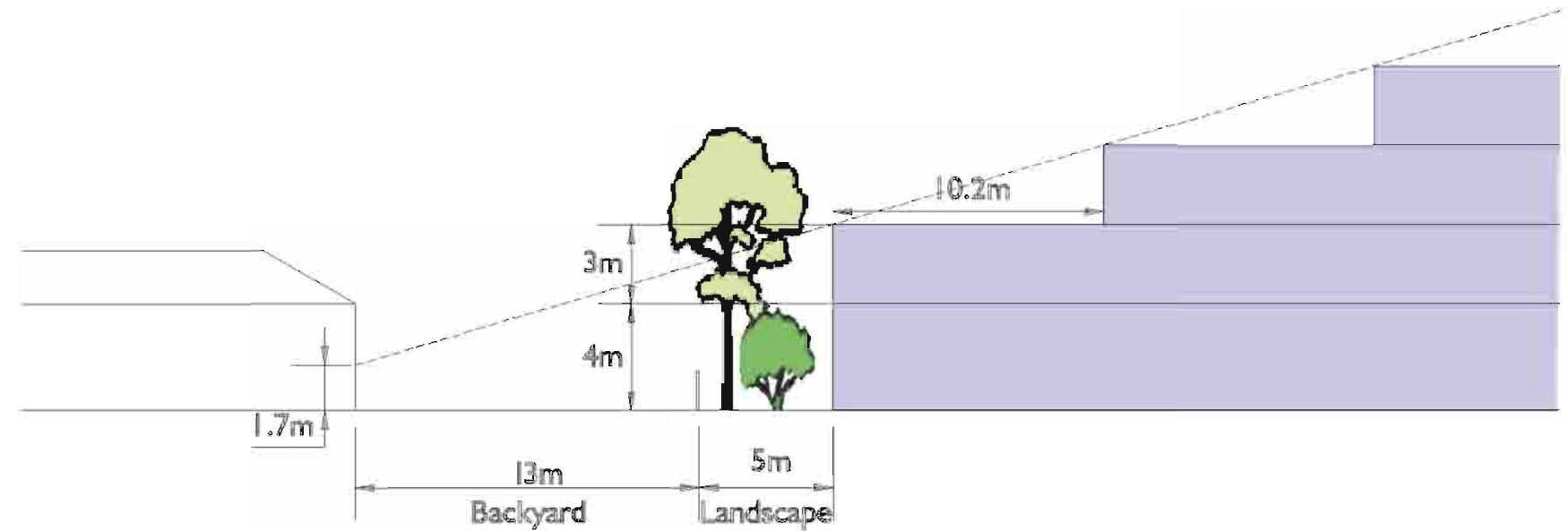


Figure 5 Setback to low scale residential interface adjacent to end-on residential lot fronting Manningtree Road and Lynch Street with landscape buffer

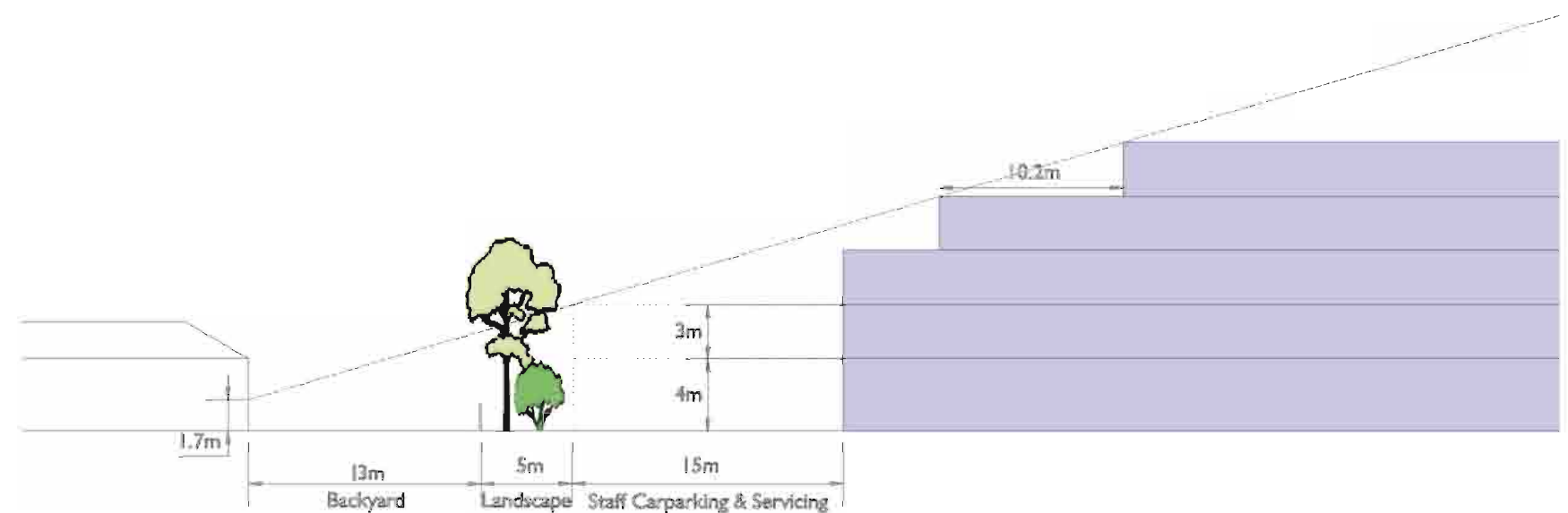


Figure 6 Setback to low scale residential interface adjacent to end-on residential lot fronting Manningtree Road and Lynch Street with landscape buffer, staff carparking and servicing area

5.0 The Future of West Hawthorn - Precinct Plans

The following section identifies how West Hawthorn is expected to develop in the future in relation to the seven precincts identified in Figure 7.

For each precinct the following elements are discussed:

- Key Objectives
- Vision
- Preferred Outcomes

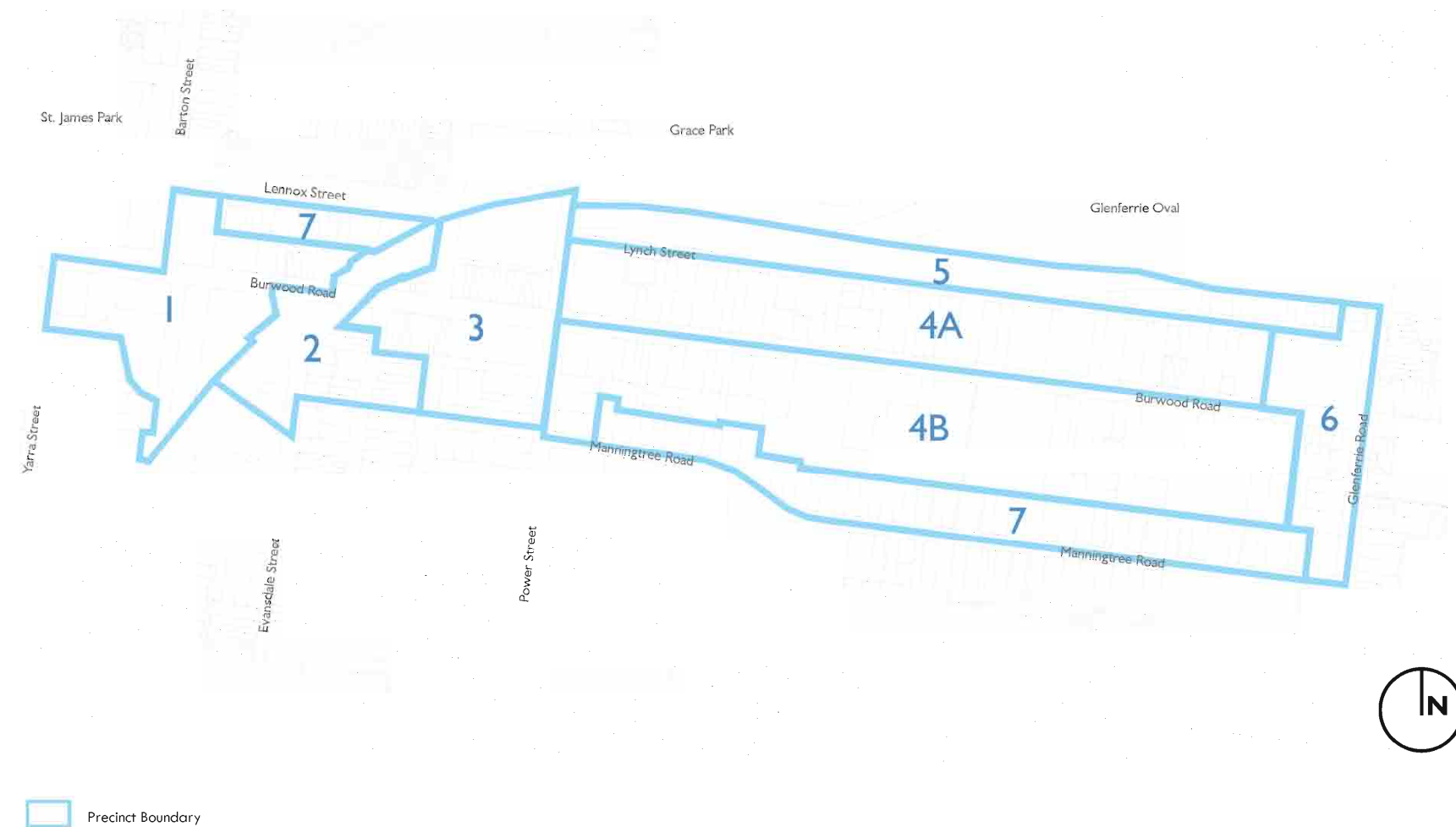
In the Preferred Outcomes section, building form is considered using the following categories:

- Low built form up to 2 storeys
- Medium built form up to 5 storeys

These building heights are indicative only. They have been defined assuming a 4.0 metre floor to ceiling height at the ground level and 3.0 metre floor to ceiling height at upper levels.

Other floor to ceiling heights may be used however the standards identified within this framework are to be adhered to regardless of the number of floors developed.

Figure 7 Precinct Plan



5.1 Precinct I Offices and Housing

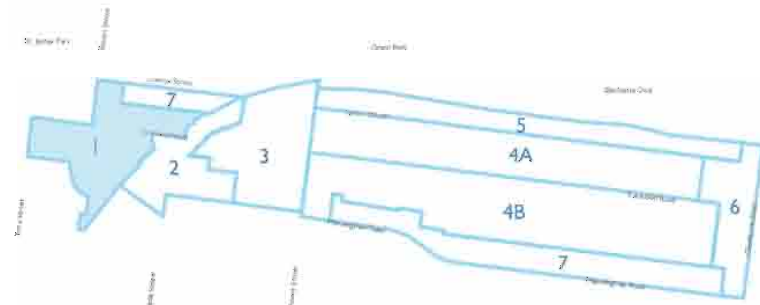


Figure 8 Precinct Plan showing Precinct I

Key Objectives

- To promote appropriate land uses
- To promote a precinct that incorporates a variety of architectural styles, high quality contemporary architecture and innovative design solutions
- To promote a built form that does not compromise the amenity of adjacent residential land uses and minimises the negative impacts of:
 - overlooking
 - overshadowing
 - noise
 - visual bulk
- To promote landscape that does not compromise the amenity of adjacent residential land uses and minimises the negative impacts of:
 - overlooking
 - poor visual amenity
- To protect and enhance public realm amenity
- To ensure energy efficient design and practices are employed
- To ensure infrastructure capacity to cater for the needs of current and future users
- To ensure infrastructure design, location and materials are to contribute to the character and amenity of all precincts

Vision

Precinct I can accommodate change. The lack of heritage buildings in this precinct and its location at the western end of West Hawthorn suggests a landmark building would be appropriate in this precinct.

Landscaping and improvement to the public realm on Burwood Road and an upgrade to the public realm through providing improved facilities for pedestrians will contribute to the character, safety and comfort of the precinct. Better connections to Hawthorn Station and Hawthorn Village will be achieved through improving the public realm and increasing legibility.

A mix of residential and office land uses will continue to be the dominant land uses in this precinct with an opportunity in time for cafes and restaurants.



Precinct I - Burwood Road corridor

Preferred Outcomes

Land Use

- Predominantly office and residential land use
- Residential land uses encouraged where adjoining land uses do not compromise amenity, safety and lifestyle

Built Form

- Medium built form
- Built form that does not compromise the amenity of adjacent residential land uses and minimises the impacts of overlooking, over shadowing, noise and visual bulk - refer to the standards contained within Clause 55 of the Planning Scheme and the City of Boroondara Residential Design Policy
- Well articulated building design that mitigates overlooking, overshadowing, noise and responds appropriately to the public realm. Refer to Guidelines for Higher Density Residential Development (DSE 2004) and Clause 55 of the Planning Scheme
- Use materials that provide improved ESD outcomes

- Facades and side walls that can be viewed from adjacent dwellings or roads are to be articulated
- Car parking and garage doors are to not dominate the streetscape
- Active frontages are to be developed along Burwood Road
- Basement car parks should be fully located below street level when fronting Burwood Road and Glenferrie Road

Building Height and Front Setback

- Buildings to be a maximum of 5 storeys in height.
- At 0.0 meters setback (the front boundary line) buildings are to be no greater than 10.0 meters in height. This results in a width to height ratio of 1:0.5 at the front boundary line
- Buildings are to be of a height and setback such that they maintain a street width to building height ratio of 1:0.5 at the front boundary line. Subsequent built form above 10.0 meters - behind the front boundary line - is to be stepped back so that it is out of sight when viewed from a height of 1.7 meters at the front boundary line on the opposite side of the street

5 Storey maximum (Ratio 1:0.5)

RATIO 1 : 0.5

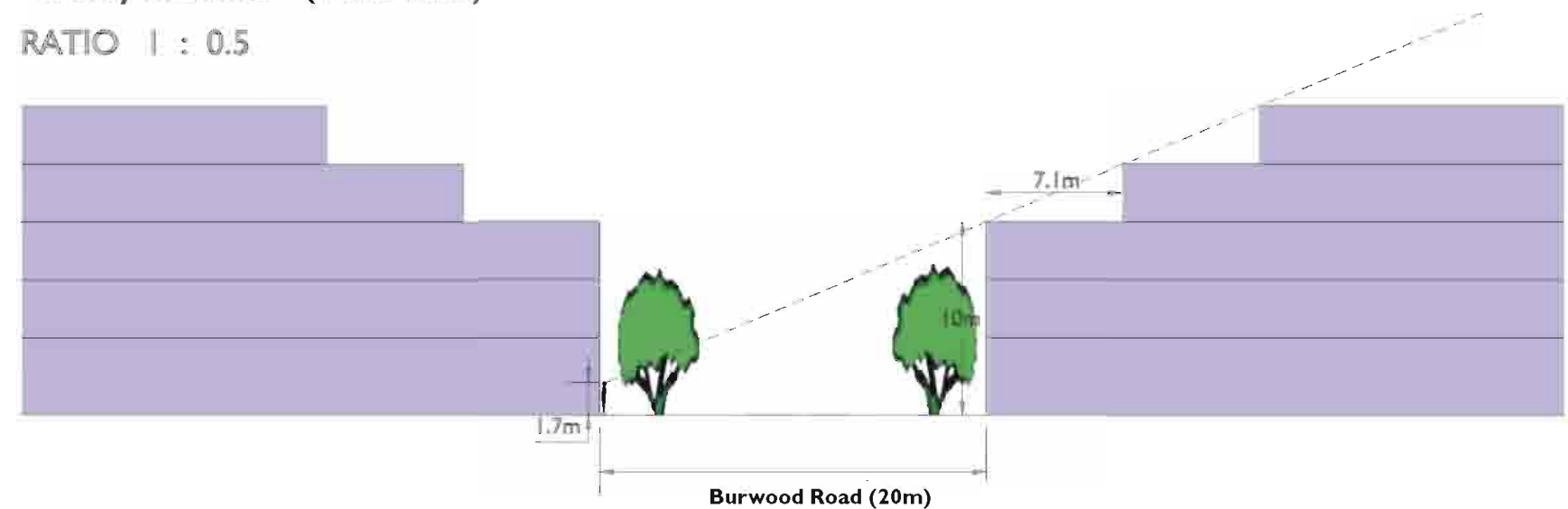


Figure 9 Front setback along Burwood Road

7m wall height at interface or staff car park and servicing

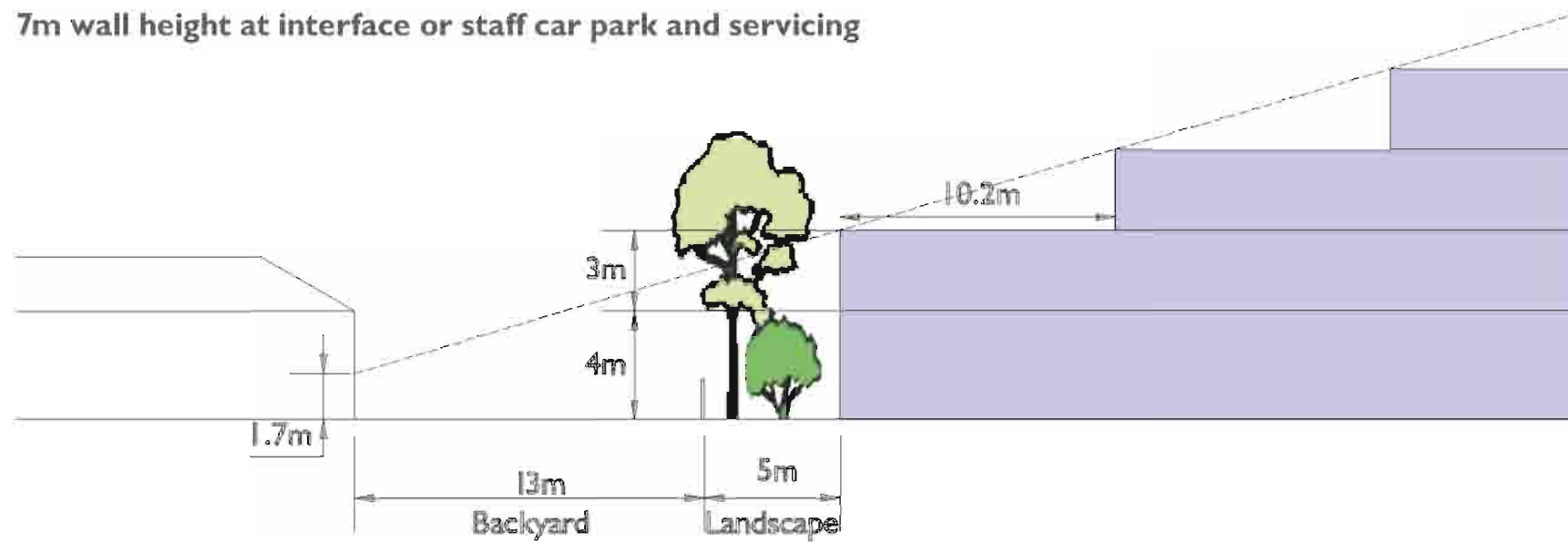


Figure 10 Setback to low scale residential interface adjacent to end-on residential lot fronting Lennox Street with landscape buffer

Side Setback

- Development on Burwood Road is to extend across the full width of the lot at least at ground floor level accept where access requirements do not allow this

Setback to Low Scale Residential Interface

See Figures 10 and 11.

- Development to be of a height that would be obscured by a building that is 7.0 meters in height and is setback 5.0 meters from the property boundary when viewed from low scale residential properties from a height of 1.7 meters at a distance of 13.0 meters from the boundary.
- Development to be setback no less than 5.0 meters from the boundary
- Introduce a significant landscape buffer of no less than 5.0 meters in width between the boundary and building. Note that the building may be set back further to allow for a staff carparking and servicing area
- Developers can choose between providing the scenario shown in Figure 10 or the scenario shown in Figure 11

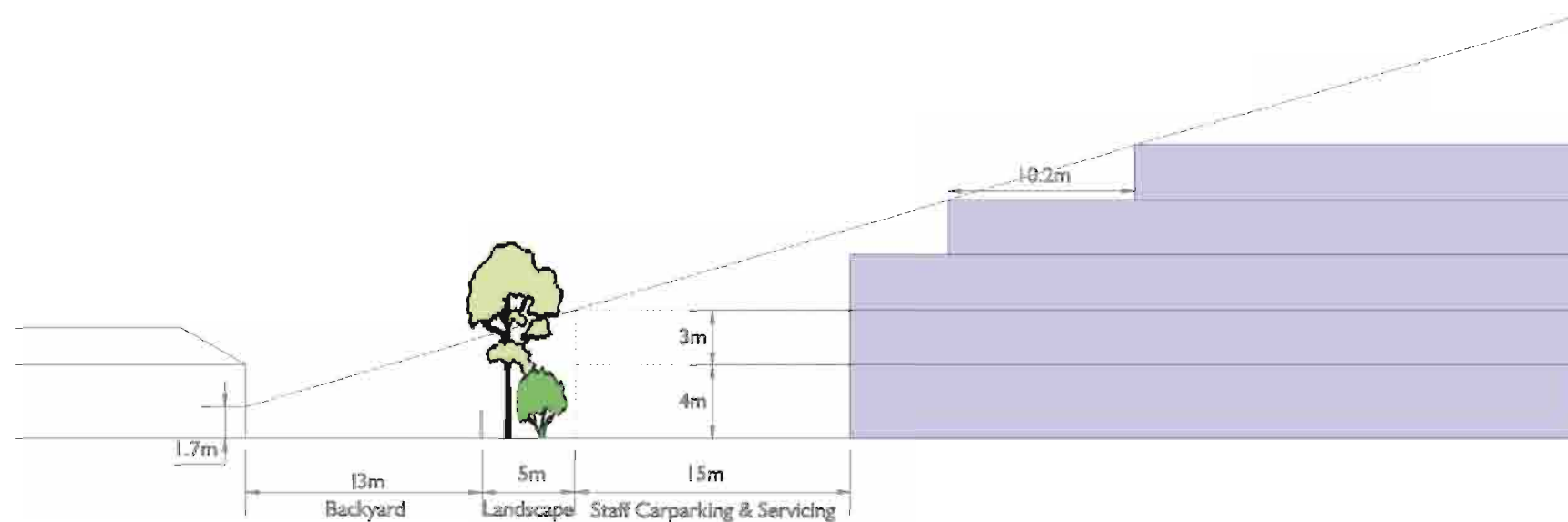


Figure 11 Setback to low scale residential interface adjacent to end-on residential lot fronting Lennox Street with landscape buffer, staff carparking and servicing area

Car Parking

- All long term staff and resident parking generated by development sites to be accommodated onsite and measures/ incentives will be provided to encourage users to use other transport modes (eg. walk, cycle and public transport)
- General standardisation of time restrictions throughout the precinct, subject to the retention of flexibility to tailor restriction times to cater for the parking activity generated by specific uses to provide consistency, and easier compliance/ enforcement
- Time limit period to be from 8:00am to 6:00pm Monday to Saturday in commercial precincts and 8:00am to 6:00pm Monday to Friday in residential areas
- Parking management and enforcement to limit non resident long term parking in nearby residential areas
- Car parking for new development to be provided in accordance with Clause 22.03 and Clause 55 of the Boroondara Planning Scheme
- New developments that prepare 'integrated transport plans' to actively encourage the use of sustainable transport modes in the precinct be eligible for reduced on-site parking provision

Movement

- Improved pedestrian access to public transport nodes
- Develop improved directional signage in the public realm for West Hawthorn
- Develop a cycling and walking strategy for West Hawthorn
- Large employers to be encouraged to develop Green Travel Plans
 - Optimise signal linking along Burwood Road
- Introduce minor extension of 'No Stopping' zones on the approach/ departure of major intersections to increase intersection capacity
- Improved information particularly at tram stops, preferably with real time transport information
- Promote and respond to programs that encourage greater use of public transport such as
 - Linking Melbourne: Metropolitan Transport Plan
 - Growing Victoria Together
 - The Sustainable State (TSS)
 - Victorian Greenhouse Strategy
- Instituting a public relations campaign to sell the benefits of public transport, including reducing road congestion, noise pollution and greenhouse gas production and to raise the status of and provide a feel good factor for those using public transport
- Explore the possibility of introducing bus services through the precinct, to fill gaps in the existing public transport network (e.g. along Power Street to Kew Junction and Burwood Road to Camberwell Junction)
- Provide upgraded tram stop amenities with seating and/ or shelter
- Direct funds obtained through parking revenues towards sustainable transport initiatives

- Improve signage to increase the general awareness of the available facilities
- Ensure pedestrian pathways comply with the DDA 1992 Improve safety and accessibility of the existing pathways and linkages through lighting, landscaping treatments, routine vegetation maintenance, widening, linemarking, and compliance with DDA provisions
- Reduce pedestrian waiting times at the existing pedestrian signals in Burwood Road. It is recommended that this be investigated through VicRoads

Public Realm

Landscape

- Landscape is to be placed in the front of buildings that are set back
- Existing mature trees are to be retained
- Heritage trees are to be protected
- Landscaping should not generally impede views of highly graded buildings
- Significant landscape is to be placed in public car parks
- Burwood Road is to become a well landscaped thoroughfare and the public realm is to be improved
- All streets are to be landscaped with avenues of trees - regular planting of significant high branching canopy trees - to soften the impacts of the built form and parking

Amenity

- Development is to occur that creates a safe pedestrian environment through design and increased surveillance – both real and perceived. See Safer Design Guidelines for Victoria, 2005
- Design for adequate sunlight on the south side of Burwood Road
- Pedestrian paths are to be kept clear to ensure view corridors are retained and real and perceived safety enhanced
- Streets are to be well lit to enhance the public safety and contribute to the night time activity of West Hawthorn
- Loading bays, site storage and waste collection is appropriately screened

Infrastructure

- New development along Burwood Road to provide underground Optical fibre cabling
- Infrastructure capacity to cater for the needs of current and future users
- An on site detention policy applicable to the redevelopment of sites within the study area restricting stormwater discharges to a specified outflow
- Infrastructure design, location and materials are to contribute to the character and amenity of all precincts

ESD

- Water use, reuse and on site management employed
- Microclimate of open spaces to be managed by positioning:-
 - Deciduous trees to the north to provide shade
 - Evergreen Vines to provide insulation
 - Use of vegetation to mitigate extremes of wind and direct required cooling breezes
- Use permeable surface coverings
- New development to provide bicycle facilities in accordance with Clause 52.34 in addition medium density residential development of more than three dwellings and office developments greater than 200sqm must provide parking of the same rates for other dwelling and office uses listed in Clause 52.34
- Incorporate Water Sensitive Urban Design where appropriate to improve quality of storm water leaving area



Utilise changes in the public realm, landscape etc, to mark the beginning of the centre and slow traffic

Built form to respect significant views corridors from both directions of Burwood Road

Landscaping and improvement to the public realm on Burwood Road

Mitigate impacts from built form and car parks along Domville Avenue, Barton Street & Yarra Street with landscaping

-  Existing heritage overlay - Retain heritage significant buildings
-  Consider an enlargement of existing heritage overlay - Protection of heritage significant buildings
-  Pedestrian access and safety enhanced
-  Indicative location for public art

Figure 12 Precinct 1

5.2 Precinct 2 Hawthorn Station Village

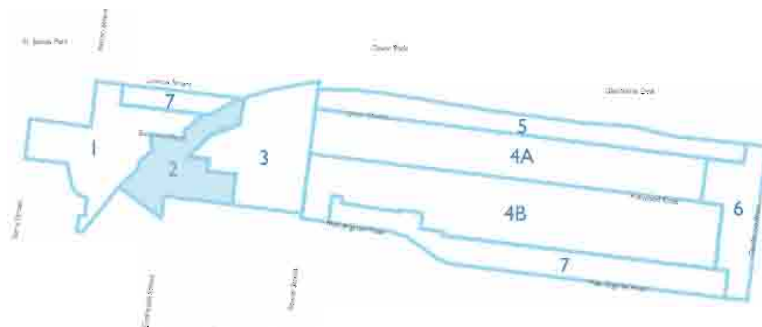


Figure 13 Precinct Plan showing Precinct 2

Key Objectives

- To promote appropriate land uses
- To protect the existing heritage character
- To promote new buildings to respect and complement adjoining heritage buildings
- To promote a building scale that reflects the village character
- To promote a built form that does not compromise the amenity of adjacent residential land uses and minimises the negative impacts of:
 - overlooking
 - overshadowing
 - noise
 - visual bulk
- To promote landscape that does not compromise the amenity of adjacent residential land uses and minimises the negative impacts of:
 - overlooking
 - poor visual amenity
- To protect and enhance public realm amenity
- To ensure energy efficient design and practices are employed
- To ensure infrastructure capacity to cater for the needs of current and future users
- To ensure infrastructure design, location and materials are to contribute to the character and amenity of all precincts

Vision

Hawthorn Station Village contains mostly single storey detached residential dwellings that have heritage significance and need to be protected and retained.

It is envisaged this precinct will continue to be a residential area with a strong character that is unique to the Boroondara area. Any change that occurs within the residential area of this precinct must occur in accordance with Res Code.

Hawthorn Station has an important local role in Hawthorn which should be reinforced through improved linkages and visual cues identifying the station's location, strengthening its significance as a transport node. The Hawthorn Station building has important heritage value which must be protected by preventing encroachment of inappropriate built form and landscape around the station building.

Opportunity for change and intensification exists along Burwood Road with a recent example of a mixed use development occurring on the south side of Burwood Road. This supports the notion that demand exists for mixed use development close to Hawthorn Village. A site exists on the northern side of Burwood Road which is appropriate for similar development.



Precinct 2 - Burwood Road and housing

Preferred Outcomes

Land Use

- Residential land use

Built Form

- Low built form
- Built form that does not compromise the amenity of adjacent residential land uses and minimises the impacts of overlooking, over shadowing, noise and visual bulk - refer to the standards contained within Clause 55 of the Planning Scheme and the City of Boroondara Residential Design Policy
- Well articulated building design that mitigates overlooking, overshadowing, noise and responds appropriately to the public realm.
- Exteriors or facades of buildings of recognized heritage value to be retained
- In heritage areas new buildings should not exceed the height of buildings on the two adjacent lots with the same street frontage
- New buildings to compliment existing heritage buildings
- In heritage areas carports and garages should be located to the rear of existing buildings
- Facades and side walls that can be viewed from adjacent dwellings or roads are to be articulated
- Car parking and garage doors are to not dominate the streetscape
- Active frontages – street frontages where there is an active visual engagement between those in the street and those on the ground floors of buildings – are to be developed along Burwood Road
- Basement car parks should be fully located below street level when fronting Burwood Road and Glenferrie Road

Side Setback

- Development on Burwood Road is to extend across the full width of the lot at least at ground floor level except where access requirements do not allow this

Car Parking

- All long term staff and resident parking generated by development sites to be accommodated onsite and measures/ incentives will be provided to encourage users to use other transport modes (eg. walk, cycle and public transport)
- General standardisation of time restrictions throughout the precinct, subject to the retention of flexibility to tailor restriction times to cater for the parking activity generated by specific uses to provide consistency, and easier compliance/ enforcement
- Time limit period to be from 8:00am to 6:00pm Monday to Saturday in commercial precincts and 8:00am to 6:00pm Monday to Friday in residential areas
- Car parking for new development to be provided in accordance with Clause 22.03 and Clause 55 of the Boroondara Planning Scheme
- Continued Parking management and enforcement to discourage long stay parking on street
- New developments that prepare 'intergrated transport plans' to actively encourage the use of sustainable transport modes in the precinct be eligible for reduced on-site parking provision
- Constrain on-site parking provision to encourage increased green travel and to protect heritage buildings

Movement

- Improved pedestrian access to public transport nodes
- Develop improved directional signage in the public realm for West Hawthorn
- Develop a cycling and walking strategy for West Hawthorn
- Large employers within the project area to be encouraged to develop Green Travel Plans
- Optimise signal linking along Burwood Road
- Improved information particularly at tram stops, preferably with real time transport information

- Promote and respond to programs that encourage greater use of public transport such as
 - Linking Melbourne: Metropolitan Transport Plan
 - Growing Victoria Together
 - The Sustainable State (TSS)
 - Victorian Greenhouse Strategy
- Instituting a public relations campaign to sell the benefits of public transport, including reducing road congestion, noise pollution and greenhouse gas production and to raise the status of and provide a feel good factor for those using public transport
- Explore the possibility of introducing bus services through the precinct, to fill gaps in the existing public transport network (e.g. along Power Street to Kew Junction and Burwood Road to Camberwell Junction)
- Provide upgraded tram stop amenities with seating and/ or shelter
- Direct funds obtained through parking revenues towards sustainable transport initiatives
- Improve signage to increase the general awareness of the available facilities
- Ensure pedestrian pathways comply with the DDA 1992
- Improve safety and accessibility of the existing pathways and linkages through lighting, landscaping treatments, routine vegetation maintenance, widening, linemarking, and compliance with DDA provisions

Public Realm

Landscape

- Landscape is to be placed in the front of buildings that are set back
- Existing mature trees are to be retained
- Heritage trees are to be protected
- Landscaping should not generally impede views of highly graded buildings
- Significant landscape is to be placed in public car parks
- Burwood Road is to become a well landscaped thoroughfare and the public realm is to be improved
- All streets are to be landscaped with avenues of trees - regular planting of significant high branching canopy trees - to soften the impacts of the built form and parking
- Public art is to be strategically located in the public realm

Amenity

- Development is to occur that creates a safe pedestrian environment through design and increased surveillance – both real and perceived. See Safer Design Guidelines for Victoria, 2005
- Design for adequate sunlight on the south side of Burwood Road
- Pedestrian paths are to be kept clear to ensure view corridors are retained and real and perceived safety enhanced
- Streets are to be well lit to enhance the public safety and contribute to the night time activity of West Hawthorn
- Loading bays, site storage and waste collection is appropriately screened
- Improve facilities at Hawthorn Station and tram stops on Burwood Road

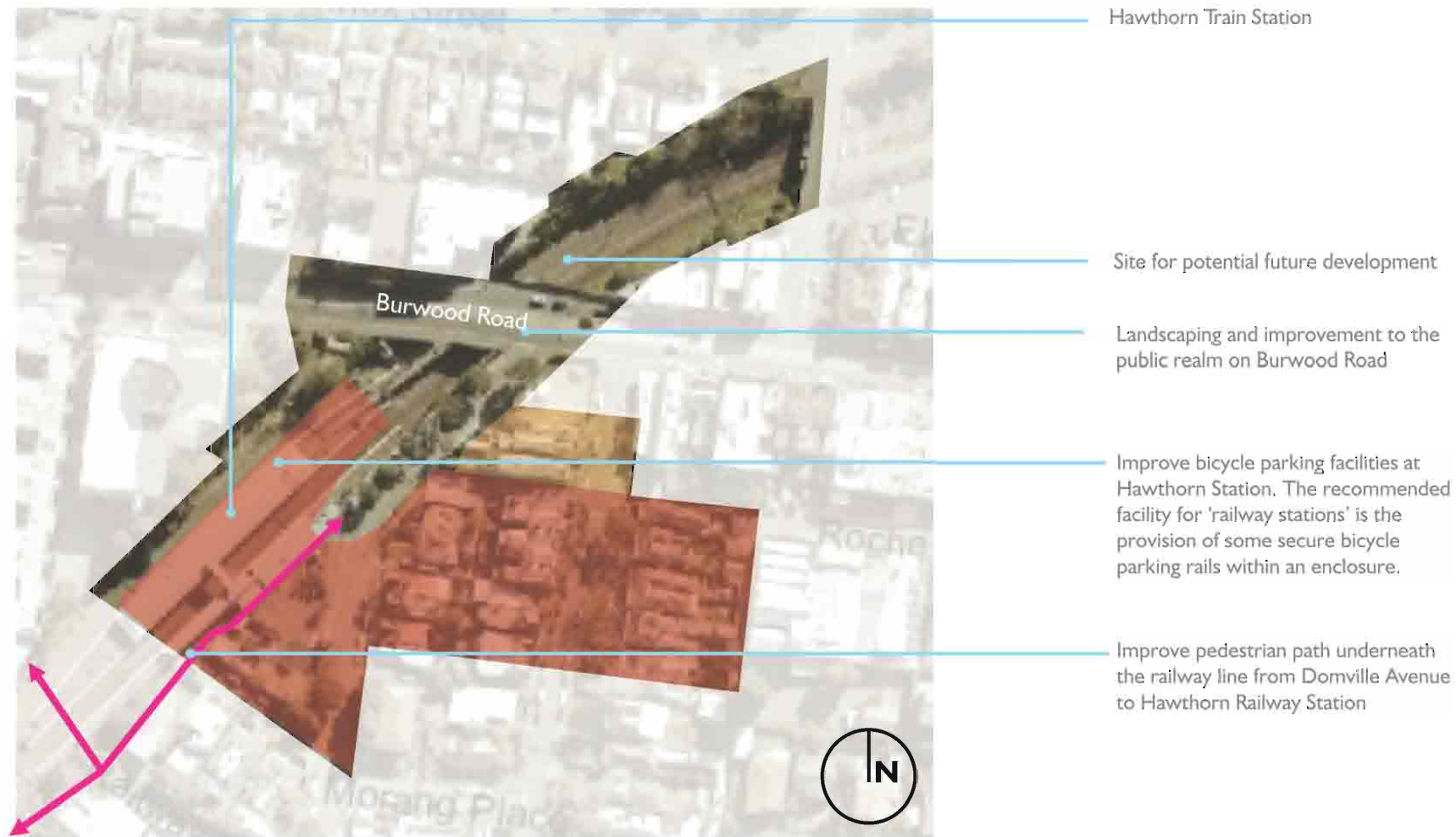
ESD

- Water use, reuse and on site management employed
- Microclimate of open spaces to be managed by positioning:-
 - Deciduous trees to the north to provide shade
 - Evergreen Vines to provide insulation
 - Use of vegetation to mitigate extremes of wind and direct required cooling breezes
- Use permeable surface coverings
- New development to provide bicycle facilities in accordance with Clause 52.34 in addition medium density residential development of more than three dwellings and office developments greater than 200sqm must provide parking of the same rates for other dwelling and office uses listed in Clause 52.34
- Incorporate Water Sensitive Urban Design where appropriate to improve quality of storm water leaving area

Infrastructure

- New development along Burwood Road to provide underground Optical fibre cabling
- Infrastructure capacity to cater for the needs of current and future users
- An on site detention policy applicable to the redevelopment of sites within the study area restricting stormwater discharges to a specified outflow
- Infrastructure design, location and materials are to contribute to the character and amenity of all precincts

Figure 14 Precinct 2



Hawthorn Train Station

Site for potential future development

Landscaping and improvement to the public realm on Burwood Road

Improve bicycle parking facilities at Hawthorn Station. The recommended facility for 'railway stations' is the provision of some secure bicycle parking rails within an enclosure.

Improve pedestrian path underneath the railway line from Domville Avenue to Hawthorn Railway Station

-  Existing heritage overlay - Retain heritage significant buildings
-  Consider an enlargement of existing heritage overlay - Protection of heritage significant buildings
-  Pedestrian access and safety enhanced
-  Indicative location for public art

5.3 Precinct 3 The West Hawthorn Village

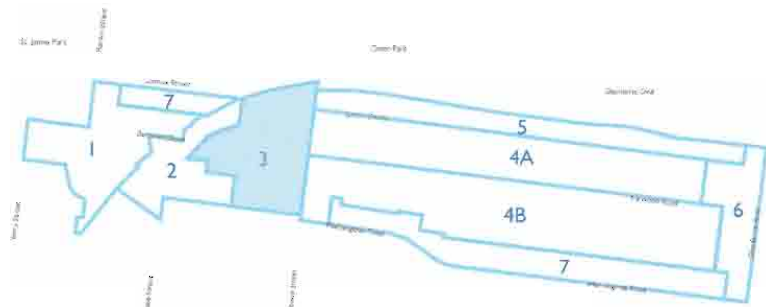


Figure 15 Precinct Plan showing Precinct 3

Key Objectives

- To promote appropriate land uses
- To promote new buildings to respect and complement heritage buildings
- To ensure building scale reflects the village character
- To promote a built form that does not compromise the amenity of adjacent residential land uses and minimises the negative impacts of:
 - overlooking
 - overshadowing
 - noise
 - visual bulk
- To promote landscape that does not compromise the amenity of adjacent residential land uses and minimises the negative impacts of:
 - overlooking
 - poor visual amenity
- To protect and enhance public realm amenity
- To ensure energy efficient design and practices are employed
- To ensure infrastructure capacity to cater for the needs of current and future users
- To ensure infrastructure design, location and materials are to contribute to the character and amenity of all precincts

Vision

West Hawthorn is well linked into the metropolitan public transport system due to its proximity to Hawthorn Station and because tram 75 runs through West Hawthorn. This is an area where intensification of the commercial and retail floor space to cater for future growth should occur. This development is to be done in such a way that it respects the built form character and heritage of the area.

Other development opportunities within the precinct that are unconstrained by sensitive adjoining land uses and/ or built form character are to be intensified according to demand.

Preferred Outcome

Land Use

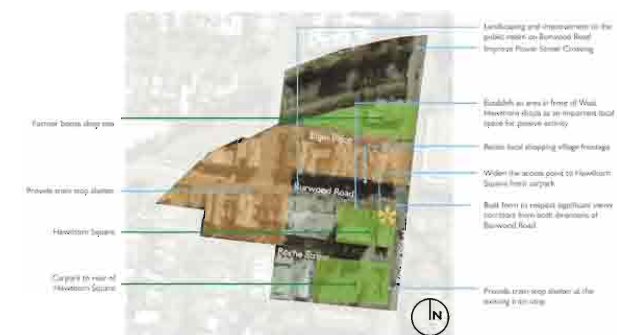
- Convenience retail and services, office and residential land uses
- Office and residential land use above ground floor
- Residential development that does not undermine the economic viability of retail, services and offices

Built Form

- Low built form with the exception of:
 - up to 3 storeys at the rear of those properties fronting Burwood Road; and
 - up to 5 storeys on the 3 identified sites:
 - Former bottle shop site
 - Hawthorn Square
 - Carpark to the rear of Hawthorn Square

Refer to Figure 19

Precinct 3 - Active edges on Burwood Road



- Built form that does not compromise the amenity of adjacent residential land uses and minimises the impacts of overlooking, over shadowing, noise and visual bulk - refer to the standards contained within Clause 55 of the Planning Scheme and the City of Boroondara Residential Design Policy
- Well articulated building design that mitigates overlooking, overshadowing, noise and responds appropriately to the public realm. Refer to Guidelines for Higher Density Residential Development (DSE 2004) and Clause 55 of the Planning Scheme
- Exteriors or facades of buildings of recognized heritage value to be retained
- In heritage areas new buildings and extensions to existing buildings should not exceed the height of buildings with heritage significance on the two adjacent lots with the same street frontage
- New buildings to complement existing heritage buildings
- In heritage areas carports and garages should be located to the rear of existing buildings
- Replacement of more than two traditional narrow buildings with a single wider building is to be discouraged
- Facades and side walls that can be viewed from adjacent dwellings or roads are to be articulated
- Carparking and garage doors are to not dominate the streetscape
- Active frontages are to be developed along Burwood Road and Power Street
- Basement car parks should be fully located below street level when fronting Burwood Road and Glenferrie Road

Building Height and Front Setback

The following outcomes apply to existing shop fronts along Burwood Road

- Development above existing shop fronts is to be set back so that it cannot be seen above the apparent street wall height when viewed from the opposite side of the street at 1.7 metres above the ground (average standing eye level)

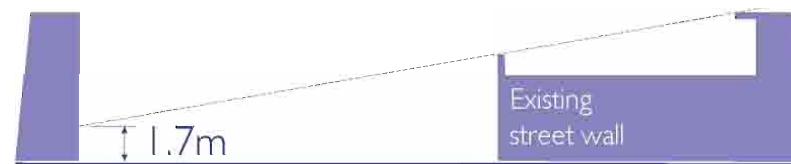


Figure 16 Example of a front setback for shoptop development that could be applied in Hawthorn Village

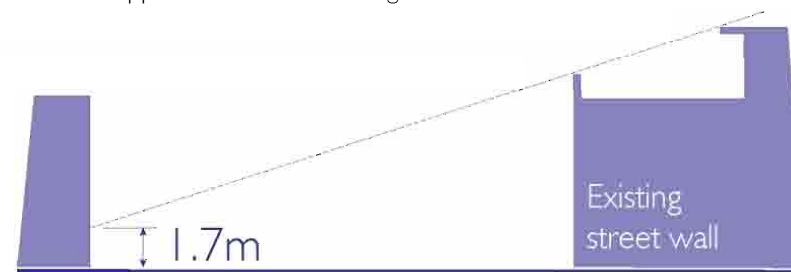


Figure 17 Example of a front setback for shoptop development that could be applied in Hawthorn Village

The following outcomes apply to 3 sites within the precinct:

- former botth shop site
- Hawthorn Square
- Carpark to the rear of Hawthorn Square

5 Storey maximum (Ratio 1:0.5)

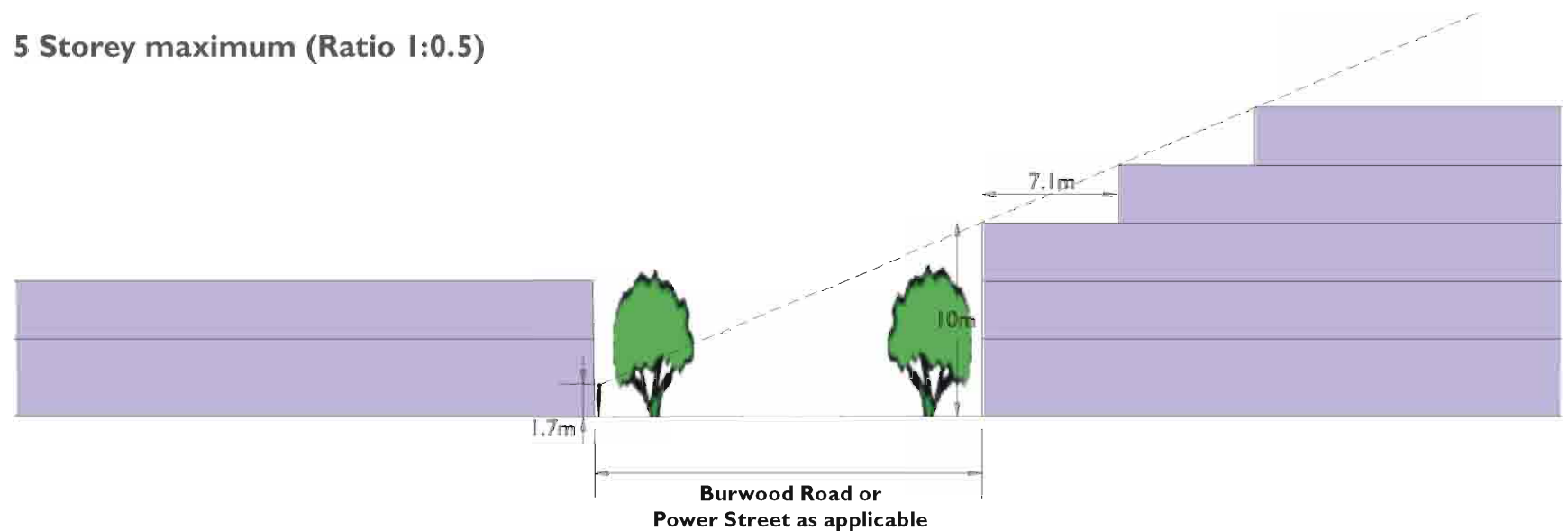


Figure 18 Front setback along Burwood Road and Power Street

See Figure 18

- Buildings to be a maximum of 5 storeys in height.
- At 0.0 meters setback (the front boundary line) buildings are to be no greater than 10.0 meters in height. This results in a width to height ratio of 1:0.5 at the front boundary line. Buildings are to be of a height and setback such that they maintain a street width to building height ratio of 1:0.5 at the front boundary line. Subsequent built form above 10.0 meters - behind the front boundary line - is to be stepped back so that it is out of sight when viewed from a height of 1.7 meters at the front boundary line on the opposite side of the street

Side Setback

- Development on Burwood Road is to extend across the full width of the lot at least at ground floor level except where access requirements do not allow this

Car Parking

- All long term staff and resident parking generated by development sites to be accommodated onsite and measures/ incentives will be provided to encourage users to use other transport modes (eg. walk, cycle and public transport)
- General standardisation of time restrictions throughout the precinct, subject to the retention of flexibility to tailor restriction times to cater for the parking activity generated by specific uses to provide consistency, and easier compliance/ enforcement
- Time limit period for on-street parking to be from 8:00am to 6:00pm Monday to Saturday in commercial precincts and 8:00am to 6:00pm Monday to Friday in residential areas
- Provision of short and limited long-stay car parking
- Additional commercial development to cater for on site staff parking with customer parking catered for nearby on and off street parking
- Car parking for new development to be provided in accordance with Clause 22.03 and Clause 55 of the Boroondara Planning Scheme
- New developments that prepare 'integrated transport plans' to actively encourage the use of sustainable transport modes in the precinct be eligible for reduced on-site parking provision

Movement

- Improved pedestrian access to public transport nodes
- Develop improved directional signage in the public realm for West Hawthorn
- Large employers within the project area to be encouraged to develop Green Travel Plans
- Develop a cycling and walking strategy for West Hawthorn
- Optimise signal linking along Burwood Road
- Introduce minor extension of 'No Stopping' zones on the approach/ departure of major intersections to increase intersection capacity

- Widen the access point to the Hawthorn Square front car park to allow shopping centre vehicles to exit the Burwood traffic stream more easily, particularly when shopping centre vehicles are also existing the car park
- Improved information particularly at tram stops, preferably with real time transport information
- Promote and respond to programs that encourage greater use of public transport such as
 - Linking Melbourne: Metropolitan Transport Plan
 - Growing Victoria Together
 - The Sustainable State (TSS)
 - Victorian Greenhouse Strategy
- Instituting a public relations campaign to sell the benefits of public transport, including reducing road congestion, noise pollution and greenhouse gas production and to raise the status of and provide a feel good factor for those using public transport
- Explore the possibility of introducing bus services through the precinct, to fill gaps in the existing public transport network (e.g. along Power Street to Kew Junction and Burwood Road to Camberwell Junction)
- Provide upgraded tram stop amenities with seating and/ or shelter
- Direct funds obtained through parking revenues towards sustainable transport initiatives
- Improve signage to increase the general awareness of the available facilities
- Ensure pedestrian pathways comply with the DDA 1992
- Improve safety and accessibility of the existing pathways and linkages through lighting, landscaping treatments, routine vegetation maintenance, widening, linemarking, and compliance with DDA provisions
- Reduce pedestrian waiting times at the existing pedestrian signals in Burwood Road. It is recommended that this be investigated through VicRoads
- Improve the on-road link across Power Street between Lennox Street and Grace Park

Public Realm

Landscape

- Landscape is to be placed in the front of buildings that are set back
- Existing mature trees are to be retained
- Heritage trees are to be protected
- Landscaping should not generally impede views of highly graded buildings
- Significant landscape is to be placed in public car parks
- Burwood Road is to become a well landscaped thoroughfare and the public realm is to be improved
- All streets are to be landscaped with avenues of trees - regular planting of significant high branching canopy trees - to soften the impacts of the built form and parking
- Public art is to be strategically located in the public realm

Figure 19 Precinct 3



- Existing heritage overlay - Retain heritage significant buildings
- Consider an enlargement of existing heritage overlay - Protection of heritage significant buildings
- Pedestrian access and safety enhanced
- Indicative location for public art
- Applicable only in this area

Amenity

- Development is to occur that creates a safe pedestrian environment through design and increased surveillance – both real and perceived. See Safer Design Guidelines for Victoria, 2005
- Design for adequate sunlight on the south side of Burwood Road
- Pedestrian paths are to be kept clear to ensure view corridors are retained and real and perceived safety enhanced
- Streets are to be well lit to enhance the public safety and contribute to the night time activity of West Hawthorn
- Loading bays, site storage and waste collection is appropriately screened

ESD

- Water use, reuse and on site management employed
- Microclimate of open spaces to be managed by positioning:-
 - Deciduous trees to the north to provide shade
 - Evergreen Vines to provide insulation
 - Use of vegetation to mitigate extremes of wind and direct required cooling breezes
- Use permeable surface coverings
- New development to provide bicycle facilities in accordance with Clause 52.34 in addition medium density residential development of more than three dwellings and office developments greater than 200sqm must provide parking of the same rates for other dwelling and office uses listed in Clause 52.34
- Incorporate Water Sensitive Urban Design where appropriate to improve quality of storm water leaving area

Infrastructure

- New development along Burwood Road to provide underground Optical fibre cabling
- Infrastructure capacity to cater for the needs of current and future users
- An on site detention policy applicable to the redevelopment of sites within the study area restricting stormwater discharges to a specified outflow
- Infrastructure design, location and materials are to contribute to the character and amenity of all precincts

5.4 Precinct 4A Mixed Use North

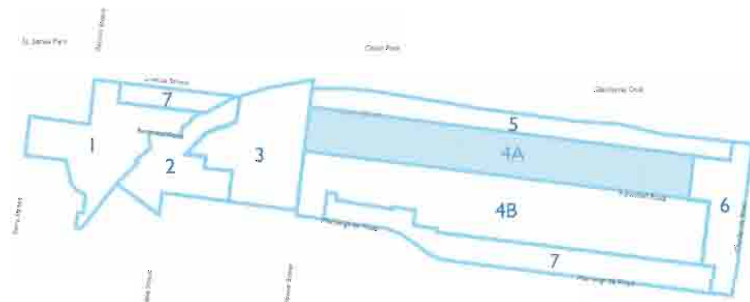


Figure 20 Precinct Plan showing Precinct 4A

Key Objectives

- To promote appropriate land uses
- To promote a precinct that incorporates a variety of architectural styles, high quality contemporary architecture and innovative design solutions
- To ensure new buildings respect and complement adjoining heritage buildings
- To promote landscape that does not compromise the amenity of adjacent residential land uses and minimises the negative impacts of:
 - overlooking
 - poor visual amenity
- To protect and enhance public realm amenity
- To ensure energy efficient design and practices are employed
- To ensure appropriate infrastructure capacity to cater for the needs of current and future users
- To ensure infrastructure design, location and materials are to contribute to the character and amenity of all precincts

Vision

The character of this precinct suggests there is significant propensity for consolidation and intensification. This is made possible because of the presence of large sites, location between the railway interface and Burwood Road and the lack of significant heritage buildings or valued built form character.

The vision for precinct 4A is therefore for a well established, large format retail shopping strip of regional importance, characterized by its concentration of bulky goods and services well located and connected with the Melbourne public transport network.

Upper levels of buildings will accommodate either office or residential development.

The built form on the southern edge of the precinct will be treated to complement high quality pedestrian environment along Burwood Road. Due to minimal constraints along Lynch Street greater intensity of development along the northern edge of the precinct is possible.

Premium views of the Glenferrie Oval and Grace Park from this side of the precinct reinforces this precincts value as a good location for higher density residential dwellings.

Links to train stations and tram stops are to be reinforced through improvements to the public realm, including the improvement of the pedestrian environment along Burwood Road.

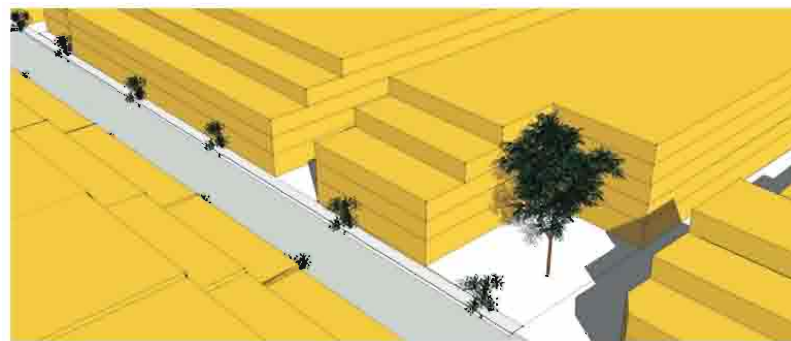


Figure 21 Looking east along Burwood Road March Equinox 3pm



Example of well articulated buildings in bulky goods/ homeware retail precinct in Richmond



Carparking being appropriately screened

Preferred Outcomes

Land Use

- Predominately office and bulky goods/ home wares shops at ground and first floor level and residential and office uses at upper levels
- Residential development that does not undermine the economic viability of the dominant land uses
- Discourage residential land uses at ground and first floor level facing Burwood Road
- Encourage an increase in business activity and floor space to capitalise on West Hawthorn's location in the Metropolitan Transport Network

Built Form

- Medium built form
- Built form that does not compromise the amenity of adjacent residential land uses and minimises the impacts of overlooking, over shadowing, noise and visual bulk - refer to the standards contained within Clause 55 of the Planning Scheme and the City of Boroondara Residential Design Policy
- Well articulated building design that mitigates overlooking, overshadowing, noise and responds appropriately to the public realm. Refer to Guidelines for Higher Density Residential Development (DSE 2004) and Clause 55 of the Planning Scheme
- Exteriors or facades of buildings of recognized heritage value to be retained
- In heritage areas new buildings should not exceed the height of buildings on the two adjacent lots with the same street frontage
- New buildings to complement existing heritage buildings
- In heritage areas carports and garages should be located to the rear of existing buildings
- Facades and side walls that can be viewed from adjacent dwellings or roads are to be articulated
- Car parking and garage doors are to not dominate the streetscape

5 Storey maximum (Ratio 1:0.5)

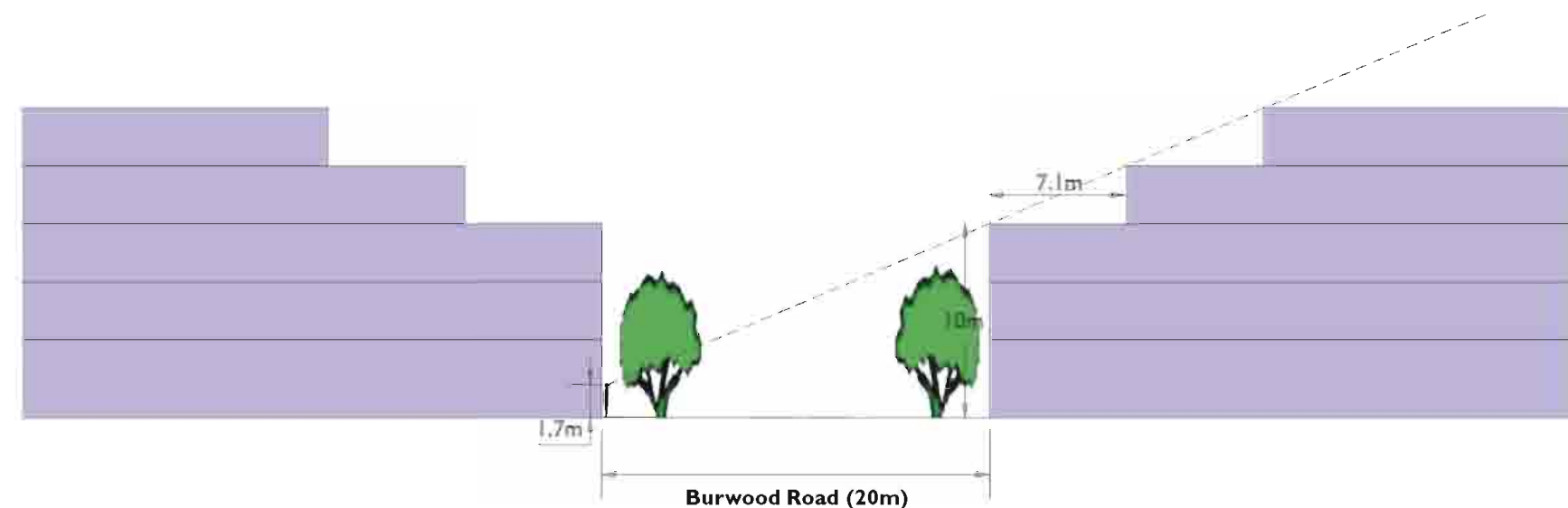


Figure 22 Front setback along Burwood Road

- Active frontages are to be developed along Burwood Road and Power Street
- Active frontages are to be developed, where it does not compromise the amenity of adjacent residential land uses with respect to noise, loss of privacy and poor visual amenity, along Tweed Street and Golding Street
- Basement car parks should be fully located below street level when fronting Burwood Road and Glenferrie Road

Side setback

- Development on Burwood Road is to extend across the full width of the lot at least at ground floor level accept where access requirements do not allow this

Building Height and Front Setback

See Figure 22

- Buildings to be a maximum of 5 storeys in height.
- At 0.0 meters setback (the front boundary line) buildings are to be no greater than 10.0 meters in height. This results in a width to height ratio of 1:0.5 at the front boundary line
- Buildings are to be of a height and setback such that they maintain a street width to building height ratio of 1:0.5 at the front boundary line. Subsequent built form above 10.0 meters - behind the front boundary line - is to be stepped back so that it is out of sight when viewed from a height of 1.7 meters at the front boundary line on the opposite side of the street

Car Parking

- All long term staff and resident parking generated by development sites to be accommodated onsite and measures/ incentives will be provided to encourage users to use other transport modes (eg. walk, cycle and public transport)
- General standardisation of time restrictions throughout the precinct, subject to the retention of flexibility to tailor restriction times to cater for the parking activity generated by specific uses to provide consistency, and easier compliance/ enforcement
- Time limit period for on-street parking to be from 8:00am to 6:00pm Monday to Saturday in commercial precincts and 8:00am to 6:00pm Monday to Friday in residential areas
- Car parking for new development to be provided in accordance with Clause 22.03 and Clause 55 of the Boroondara Planning Scheme
- New developments that prepare 'integrated transport plans' to actively encourage the use of sustainable transport modes in the precinct be eligible for reduced on-site parking provision

Movement

- Optimise signal linking along Burwood Road
- Introduce minor extension of 'No Stopping' zones on the approach/ departure of major intersections to increase intersection capacity
- Install a pedestrian crossing facility mid-block between Glenferrie Road and Power Street
- Create new vehicular access connections to sites from side streets (Tweed Street and Golding Street) and Lynch Street, with Lynch Street continuing to provide access to staff parking areas/ back of house/ service areas, and be available for short term customer goods pick-up
- Do not support new vehicular access connections off Burwood Road for site ingress due to traffic disturbance along Burwood Road (likely to be refused by VicRoads if other access options available)
- Some egress only vehicular connections onto Burwood Road but the number is to be limited to maintain the integrity of the streetscape along Burwood Road

- Promote and respond to programs that encourage greater use of public transport such as
 - Linking Melbourne: Metropolitan Transport Plan
 - Growing Victoria Together
 - The Sustainable State (TSS)
 - Victorian Greenhouse Strategy
 - Extend tram route along Burwood Road from Power Street to Glenferrie Road
 - provide tram stop amenities with seating and/ or shelter
- Instituting a public relations campaign to sell the benefits of public transport, including reducing road congestion, noise pollution and greenhouse gas production and to raise the status of and provide a feel good factor for those using public transport
- Explore the possibility of introducing bus services through the precinct, to fill gaps in the existing public transport network (e.g. along Power Street to Kew Junction and Burwood Road to Camberwell Junction)
- Direct funds obtained through parking revenues towards sustainable transport initiatives
- Improve signage to increase the general awareness of the available facilities
- Ensure pedestrian pathways comply with the DDA 1992
- Improve safety and accessibility of the existing pathways and linkages through lighting, landscaping treatments, routine vegetation maintenance, widening, linemarking, and compliance with DDA provisions
- Develop improved directional signage in the public realm for West Hawthorn
- Develop a cycling and walking strategy for West Hawthorn
- Large employers within the project area to be encouraged to develop Green Travel Plans



Public Realm

Landscape

- Landscape is to be placed in the front of buildings that are set back
- Existing mature trees are to be retained
- Heritage trees are to be protected
- Landscaping should not generally impede views of highly graded buildings

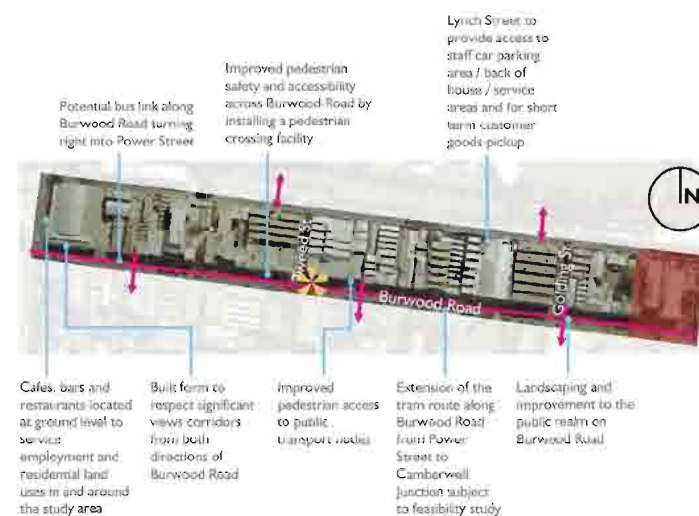


Highly articulated residential development

- Significant landscape is to be placed in public car parks
- Burwood Road is to become a well landscaped thoroughfare and the public realm is to be improved
- All streets are to be landscaped with avenues of trees - regular planting of significant high branching canopy trees - to soften the impacts of the built form and parking
- Public art is to be strategically located in the public realm

Amenity

- Development is to occur that creates a safe pedestrian environment through design and increased surveillance – both real and perceived. See Safer Design Guidelines for Victoria, 2005
- Design for adequate sunlight on the south side of Burwood Road
- Pedestrian paths are to be kept clear to ensure view corridors are retained and real and perceived safety enhanced
- Streets are to be well lit to enhance the public safety and contribute to the night time activity of West Hawthorn
- Loading bays, site storage and waste collection is appropriately screened



Precinct 4A - Views west along Burwood Road



Precinct 4A - Poor public realm quality



Contemporary large format retail development on Bridge Road, Richmond

ESD

- Water use, reuse and on site management employed
- Microclimate of open spaces to be managed by positioning:
 - Deciduous trees to the north to provide shade
 - Evergreen Vines to provide insulation
 - Use of vegetation to mitigate extremes of wind and direct required cooling breezes
- Use permeable surface coverings
- New development to provide bicycle facilities in accordance with Clause 52.34 in addition medium density residential development of more than three dwellings and office developments greater than 200sqm must provide parking of the same rates for other dwelling and office uses listed in Clause 52.34
- Incorporate Water Sensitive Urban Design where appropriate to improve quality of storm water leaving area

Infrastructure

- New development along Burwood Road to provide underground Optical fibre cabling
- Infrastructure capacity to cater for the needs of current and future users
- An on site detention policy applicable to the redevelopment of sites within the study area restricting stormwater discharges to a specified outflow
- Infrastructure design, location and materials are to contribute to the character and amenity of all precincts

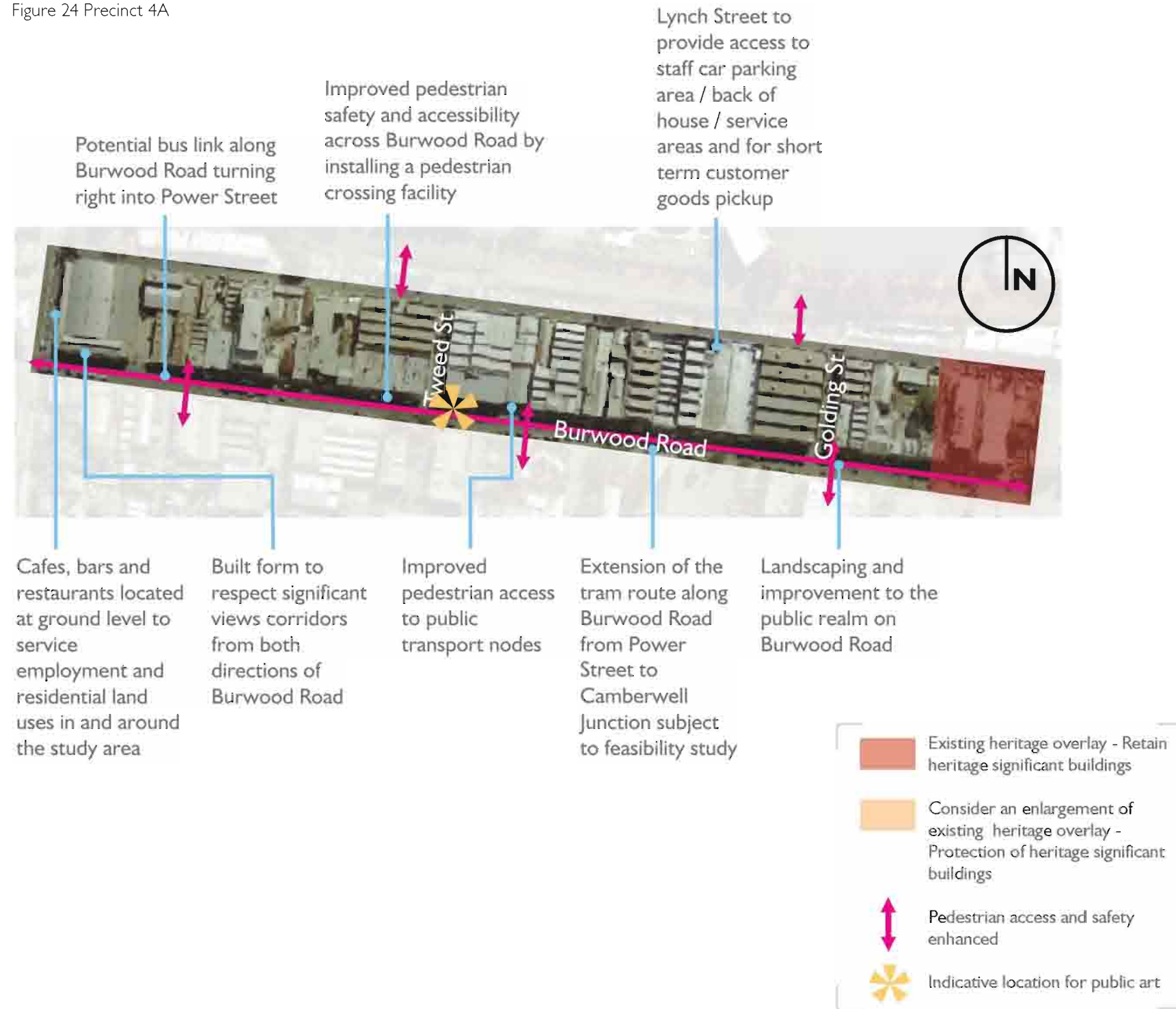


Figure 23 Looking east along Lynch Street towards Glenferrie Road



“Back door” character functioning as service corridor for a variety of land uses with residential development at upper levels

Figure 24 Precinct 4A



5.5 Precinct 4B Mixed Use South

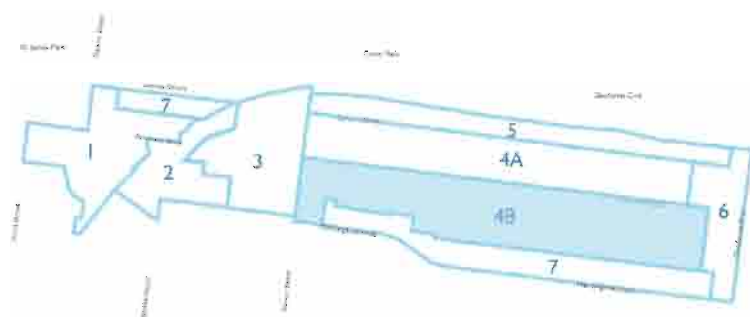


Figure 25 Precinct Plan showing Precinct 4B

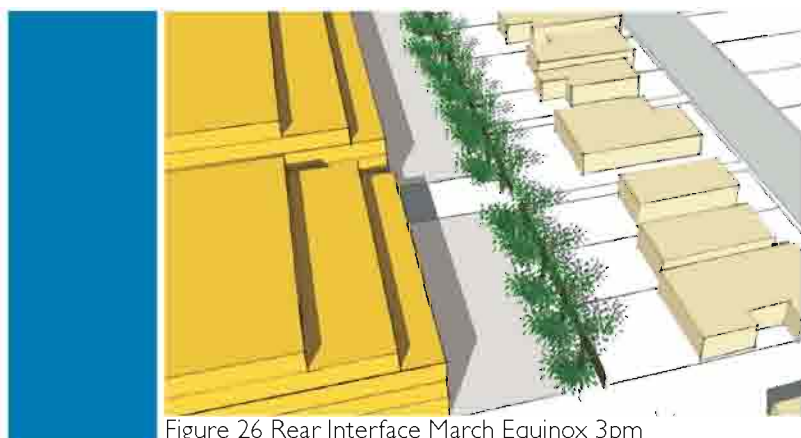


Figure 26 Rear Interface March Equinox 3pm



Example of highly articulated built form in a good quality pedestrian environment in Elwood

Key Objectives

- To promote appropriate land uses
- To promote the future development of a precinct that incorporates a variety of architectural styles, high quality contemporary architecture and innovative design solutions
- To ensure new buildings respect and complement adjoining heritage buildings
- To promote a built form that does not compromise the amenity of adjacent residential land uses and minimises the negative impacts of:
 - overlooking
 - overshadowing
 - noise
 - visual bulk
- To promote landscape that does not compromise the amenity of adjacent residential land uses and minimises the negative impacts of:
 - overlooking
 - poor visual amenity
- To protect and enhance the amenity of the public realm
- To ensure energy efficient design and practices are employed
- To ensure infrastructure capacity to cater for the needs of current and future users
- To ensure infrastructure design, location and materials are to contribute to the character and amenity of all precincts

Vision

This precinct has significant potential for change and intensification. Large property envelopes and the precincts proximity to transport nodes contribute to the areas potential to become an important regional centre for bulky goods.

The vision for precinct 4B is for a well established, large format retail shopping strip of regional importance, characterized by its concentration of bulky goods and services well located and connected with the Melbourne public transport network.

Upper levels above the bulky good land uses will accommodate either office or residential development.

This precinct will incorporate a variety of architectural style, high quality architecture and provide innovative design solutions to mitigate any impacts on adjoining sensitive land uses.

The level and intensity of development within this precinct is constrained by the sensitive residential interface with the Manningtree Road properties on its southern boundary. Residential amenity is to be improved through adherence to the City of Boroondara Residential Design Policy. Acoustic treatments and a landscape buffer that softens the impact of built form and commercial activity on adjoining lots is to be incorporated along all residential interfaces.

Links to both stations and trams stops are to be reinforced through improvements to the public realm, including the landscaping and improvement to the public realm on Burwood Road.



An example of active frontages at ground level with residential development at upper levels

Preferred Outcomes

Land Use

- Residential development that does not undermine the economic viability of the dominant land uses
- Predominately office and bulky goods/ home wares shops at ground and first floor level and residential and office use at upper levels
- Discourage residential land uses at ground and first floor level facing Burwood Road
- Encourage an increase in business activity and floor space to capitalise on West Hawthorns location in the Metropolitan Transport Network

Built Form

- Medium built form
- Built form that does not compromise the amenity of adjacent residential land uses and minimises the impacts of overlooking, over shadowing, noise and visual bulk - refer to the standards contained within Clause 55 of the Planning Scheme

5 Storey maximum (Ratio 1:0.5)

RATIO 1 : 0.5

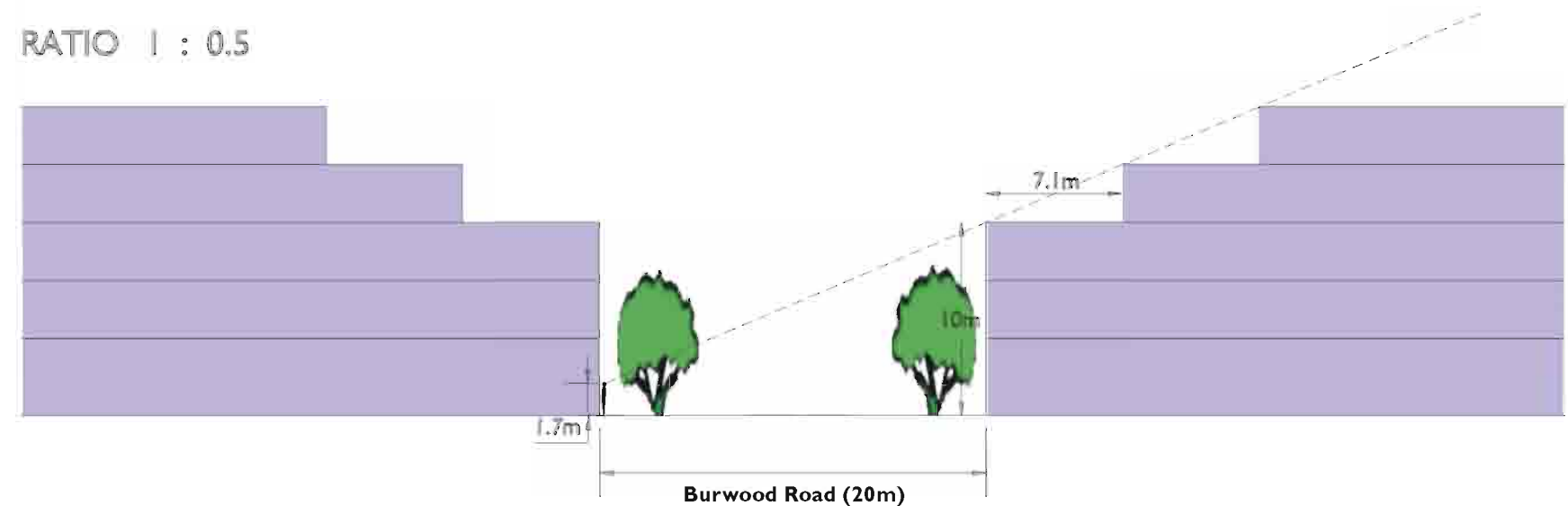


Figure 27 Front setback on Burwood Road

- and the City of Boroondara Residential Design Policy
- Well articulated building design that mitigates overlooking, overshadowing, noise and responds appropriately to the public realm. Refer to Guidelines for Higher Density Residential Development (DSE 2004) and Clause 55 of the Planning Scheme
- Facades and side walls that can be viewed from adjacent dwellings or roads are to be articulated
- Car parking and garage doors are to not dominate the streetscape
- Active frontages are to be developed along Burwood Road and Power Street
- Basement car parks should be fully located below street level when fronting Burwood Road and Glenferrie Road
- Active frontages are to be developed, where it does not compromise the amenity of adjacent residential land uses with respect to noise, loss of privacy and poor visual amenity, along Lavidge Street, Thomas Street, Drill Street, Elizabeth Street, Guest Street, Cook Street and Lutton Lane

Building Height and Front Setback

See Figure 27.

- Buildings to be a maximum of 5 storeys in height.
- At 0.0 meters setback (the front boundary line) buildings are to be no greater than 10.0 meters in height. This results in a width to height ratio of 1:0.5 at the front boundary line
- Buildings are to be of a height and setback such that they maintain a street width to building height ratio of 1:0.5 at the front boundary line. Subsequent built form above 10.0 meters - behind the front boundary line - is to be stepped back so that it is out of sight when viewed from a height of 1.7 meters at the front boundary line on the opposite side of the street

Side Setback

- Development on Burwood Road is to extend across the full width of the lot at least at ground floor level except where access requirements do not allow this

Setback to Low Scale Residential Interface

See Figures 28 and 29.

- Development to be of a height that would be obscured by a building that is 7.0 meters in height and is setback 5.0 meters from the property boundary when viewed from low scale residential properties from a height of 1.7 meters at a distance of 13.0 meters from the boundary.
- Development to be setback no less than 5.0 meters from the boundary.
- Introduce a significant landscape buffer of no less than 5.0 meters in width between the boundary and building. Note that the building may be set back further to allow for a staff carparking and servicing area
- Developers can choose between providing the scenario shown in Figure 28 or the scenario shown in Figure 29

7m wall height at interface or staff car park and servicing

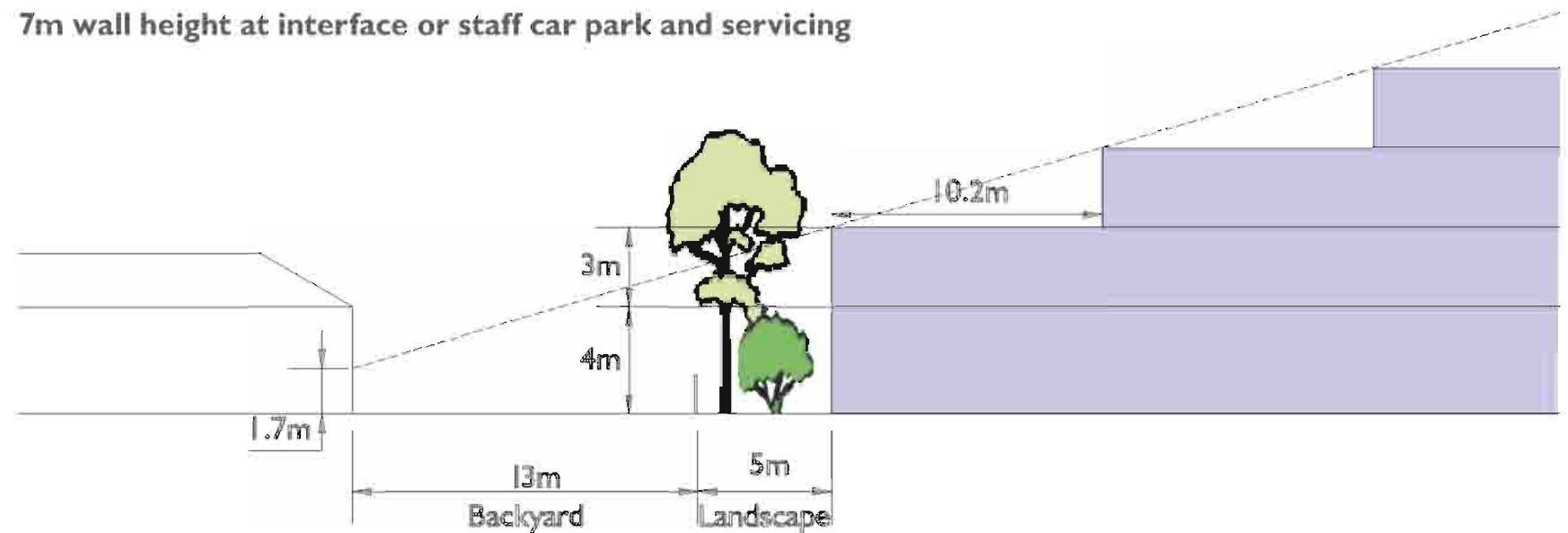


Figure 28 Setback to low scale residential interface adjacent to end-on residential lot fronting Manningtree Road with landscape buffer

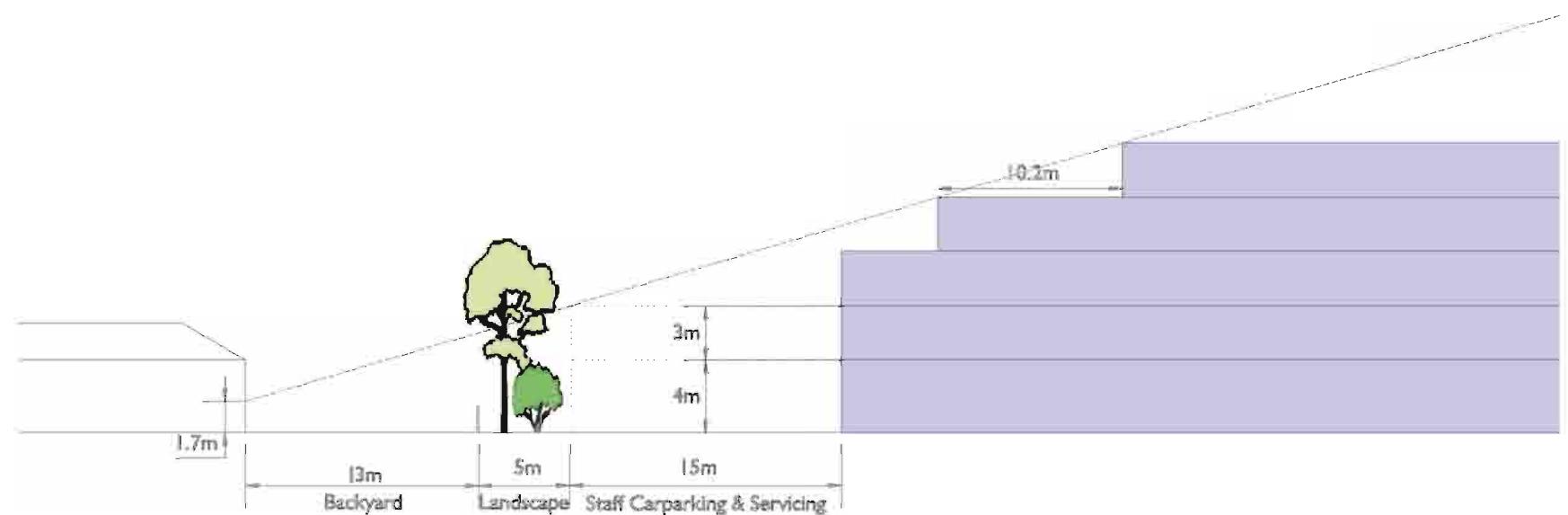
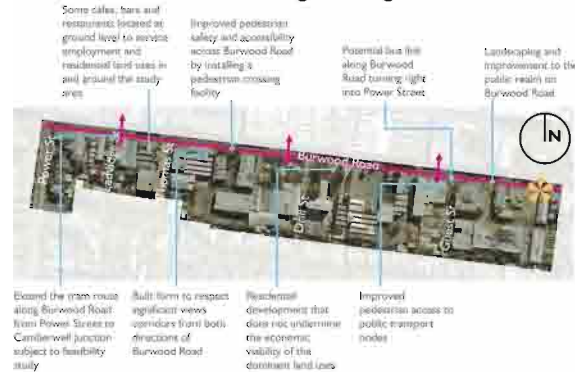


Figure 29 Setback to low scale residential interface adjacent to end-on residential lot fronting Manningtree Road with landscape buffer, Staff carparking and servicing

Precinct 4B - Active frontages along Burwood Road



Car Parking

- All long term staff and resident parking generated by development sites to be accommodated onsite and measures/ incentives will be provided to encourage users to use other transport modes (eg. walk, cycle and public transport)
- General standardisation of time restrictions throughout the precinct, subject to the retention of flexibility to tailor restriction times to cater for the parking activity generated by specific uses (eg Capers and the Elgin Inn) to provide consistency, and easier compliance/ enforcement
- Time limit period to be from 8:00am to 6:00pm Monday to Saturday in commercial precincts and 8:00am to 6:00pm Monday to Friday in residential areas

- Larger showrooms/ bulky goods premises should seek to provide on-site parking for staff and customers at basement level
- Car parking for new development to be provided in accordance with Clause 22.03 and Clause 55 of the Boroondara Planning Scheme
- New developments that prepare 'integrated transport plans' to actively encourage the use of sustainable transport modes in the precinct be eligible for reduced on-site parking provision

Movement

- Optimise signal linking along Burwood Road
- Introduce minor extension of 'No Stopping' zones on the approach/ departure of major intersections to increase intersection capacity
- Install a pedestrian crossing facility on Burwood Road mid-block between Glenferrie Road and Power Street
- Development sites located midblock along Burwood Road to be accessed directly via Burwood Road and consider the use of Drill Street for controlled access to abutting sites with frontage to Burwood Road, subject to appropriate traffic management controls to minimise any traffic impacts onto Manningtree Road
- Minimise the number of new vehicular access connections onto Burwood Road for site ingress/ egress
- Develop improved directional signage in the public realm for West Hawthorn
- Develop a cycling and walking strategy for West Hawthorn
- Large employers within the project area to be encouraged to develop Green Travel Plans
- Improved information particularly at tram stops, preferably with real time transport information
- Promote and respond to programs that encourage greater use of public transport such as
 - Linking Melbourne: Metropolitan Transport Plan
 - Growing Victoria Together
 - The Sustainable State (TSS)

- Victorian Greenhouse Strategy
- Extend tram route along Burwood Road from Power Street to Glenferrie Road
 - provide tram stop amenities with seating and/ or shelter
- Instituting a public relations campaign to sell the benefits of public transport, including reducing road congestion, noise pollution and greenhouse gas production and to raise the status of and provide a feel good factor for those using public transport
- Explore the possibility of introducing bus services through the precinct, to fill gaps in the existing public transport network (e.g. along Power Street to Kew Junction and Burwood Road to Camberwell Junction)
- Direct funds obtained through parking revenues towards sustainable transport initiatives
- Improve signage to increase the general awareness of the available facilities
- Ensure pedestrian pathways comply with the DDA 1992
- Improve safety and accessibility of the existing pathways and linkages through lighting, landscaping treatments, routine vegetation maintenance, widening, linemarking, and compliance with DDA provisions
- Reduce pedestrian waiting times at the existing pedestrian signals in Burwood Road. It is recommended that this be investigated through VicRoads

Public Realm

Landscape

- Provide a landscape buffer that when fully matured substantially screen buildings adjoining the Manningtree Road properties
- Landscape is to be placed in the front of buildings that are set back
- Existing mature trees are to be retained
- Heritage trees are to be protected
- Landscaping should not generally impede views of highly

graded buildings

- Significant landscape is to be placed in public car parks
- Burwood Road is to become a well landscaped thoroughfare and the public realm is to be improved
- All streets are to be landscaped with avenues of trees - regular planting of significant high branching canopy trees - to soften the impacts of the built form and parking
- Public art is to be strategically located in the public realm

Amenity

- Development is to occur that creates a safe pedestrian environment through design and increased surveillance – both real and perceived. See Safer Design Guidelines for Victoria, 2005
- Pedestrian paths are to be kept clear to ensure view corridors are retained and real and perceived safety enhanced
- Streets are to be well lit to enhance the public safety and contribute to the night time activity of West Hawthorn

ESD

- Water use, reuse and on site management employed
- Microclimate of open spaces to be managed by positioning:-
 - Deciduous trees to the north to provide shade
 - Evergreen Vines to provide insulation
 - Use of vegetation to mitigate extremes of wind and direct required cooling breezes
- Use permeable surface coverings
- New development to provide bicycle facilities in accordance with Clause 52.34 in addition medium density residential development of more than three dwellings and office developments greater than 200sqm must provide parking of the same rates for other dwelling and office uses listed in Clause 52.34
- Incorporate Water Sensitive Urban Design where appropriate to improve quality of storm water leaving area

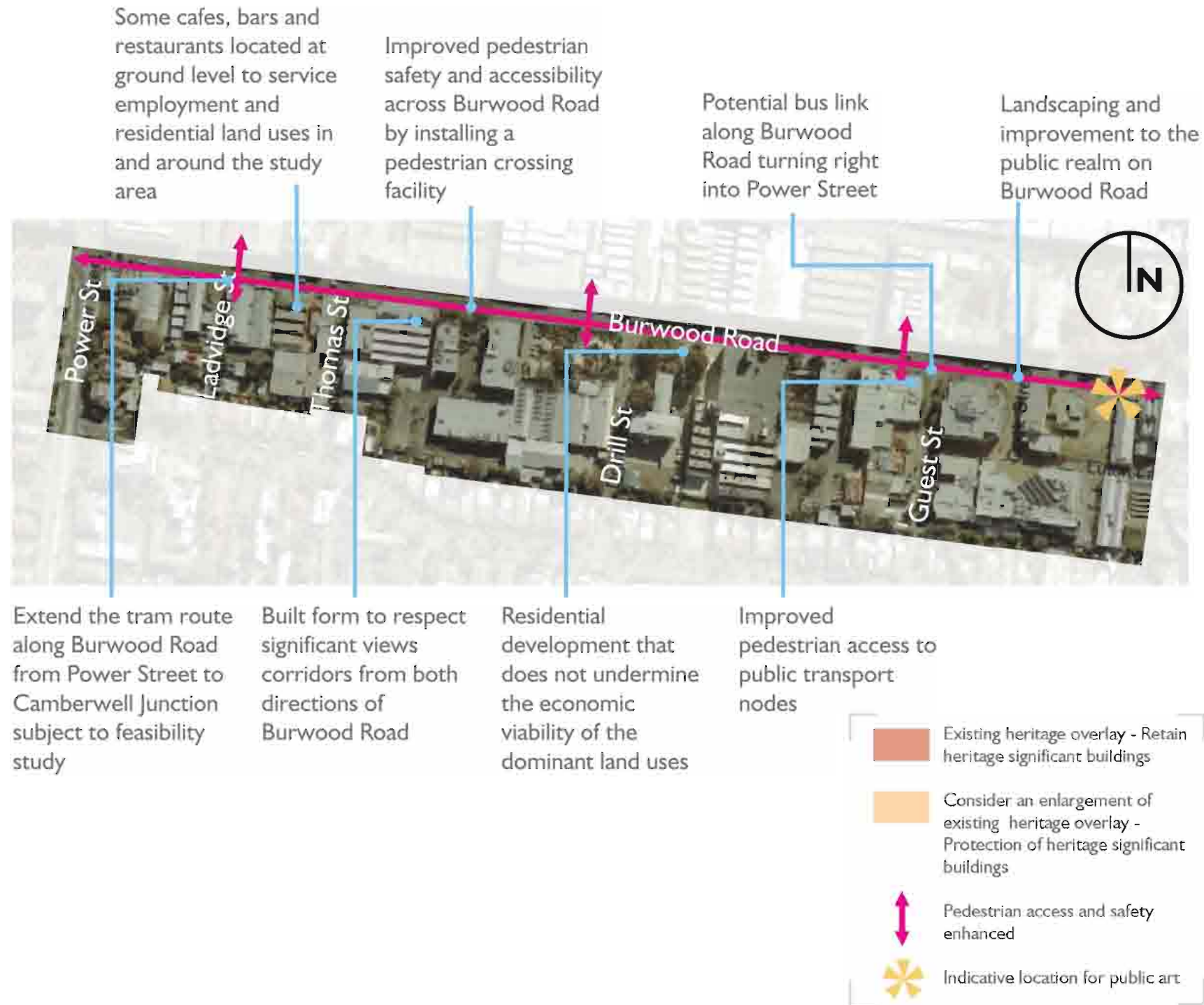


Figure 30 Burwood Road September Equinox 3pm

Infrastructure

- New development along Burwood Road to provide underground Optical fibre cabling
- Infrastructure capacity to cater for the needs of current and future users
- An on site detention policy applicable to the redevelopment of sites within the study area restricting stormwater discharges to a specified outflow
- Infrastructure design, location and materials are to contribute to the character and amenity of all precincts

Figure 31 Precinct 4B



5.6 Precinct 5 Transit Oriented Development

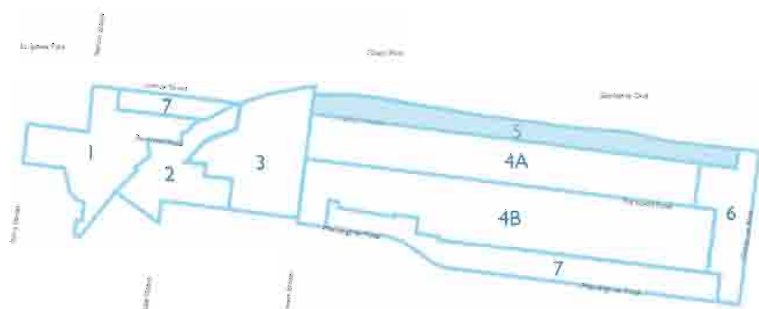


Figure 32 Precinct Plan showing Precinct 5



Poor quality Public Realm in Lynch Street



Figure 33 Looking east along Lynch Street towards Glenferrie Road

Key Objectives

- To promote appropriate land uses
- To promote a precinct that incorporates a variety of architectural styles, high quality contemporary architecture and innovative design solutions
- To promote landscape that does not compromise the amenity of adjacent residential land uses and minimises the negative impacts of:
 - overlooking
 - poor visual amenity
- To protect and enhance public realm amenity
- To ensure energy efficient design and practices employed
- To ensure infrastructure capacity to cater for the needs of current and future users
- To ensure Infrastructure design, location and materials are to contribute to the character and amenity of all precincts
- To improve linkages to parkland

Vision

Lynch Street functions as a service corridor for a variety of large format land uses which mostly front Burwood Road.

The vision for the precinct is to activate Lynch Street by locating townhouses at both the eastern and western ends of the precinct and introducing a built form character that incorporates a variety of architectural styles, high quality contemporary architecture and innovative design solutions.

The public realm along Lynch Street will be improved through increased tree planting, street furniture and lighting to the quality and safety of the precinct.

- Maintain the service role that Lynch Street has to service existing buildings
- Linkages to Grace Park under railway

Preferred Outcomes

Land Use

- Residential land use
- Office land use

Built Form

- Low built form
- Well articulated building design that mitigates overlooking, overshadowing, noise and responds appropriately to the public realm
- Exteriors or facades of buildings of recognized heritage value to be retained
- In heritage areas new buildings should not exceed the height of buildings on the two adjacent lots with the same street frontage
- New buildings to complement existing heritage buildings
- In heritage areas carports and garages should be located to the rear of existing buildings
- Facades and side walls that can be viewed from adjacent dwellings or roads are to be articulated
- Car parking and garage doors are to not dominate the streetscape

Carparking

- All long term staff and resident parking generated by development sites to be accommodated onsite and measures/ incentives to be provided to encourage users to use other transport modes (eg. walk, cycle and public transport)
- General standardisation of time restrictions throughout the precinct, subject to the retention of flexibility to tailor restriction times to cater for the parking activity generated by specific uses (eg Capers and the Elgin Inn) to provide consistency, and easier compliance/ enforcement
- Time limit period for on-street parking to be from 8:00am to 6:00pm Monday to Saturday in commercial precincts and 8:00am to 6:00pm Monday to Friday in residential areas

- Residential development to meet the resident rates of Clause 55.03 – 11 of the Boroondara Planning Scheme, with an allowance for off-street visitor parking for suitably designed dwellings
- Car parking for new development to be provided in accordance with Clause 22.03 and Clause 55 of the Boroondara Planning Scheme
- New developments that prepare 'integrated transport plans' to actively encourage the use of sustainable transport modes in the precinct be eligible for reduced on-site parking provision

Movement

- Develop a cycling and walking strategy for West Hawthorn
- Develop improved directional signage in the public realm for West Hawthorn
- Large employers within the project area to be encouraged to develop Green Travel Plans
- Improved information particularly at tram stops, preferably with real time transport information
- Promote and respond to programs that encourage greater use of public transport such as
 - Linking Melbourne: Metropolitan Transport Plan
 - Growing Victoria Together
 - The Sustainable State (TSS)
 - Victorian Greenhouse Strategy
- Instituting a public relations campaign to sell the benefits of public transport, including reducing road congestion, noise pollution and greenhouse gas production and to raise the status of and provide a feel good factor for those using public transport
- Explore the possibility of introducing bus services through the precinct, to fill gaps in the existing public transport network (e.g. along Power Street to Kew Junction and Burwood Road to Camberwell Junction)
- Direct funds obtained through parking revenues towards sustainable transport initiatives
- Improve signage to increase the general awareness of the available facilities

- Ensure pedestrian pathways comply with the DDA 1992
- Improve safety and accessibility of the existing pathways and linkages through lighting, landscaping treatments, routine vegetation maintenance, widening, linemarking, and compliance with DDA provisions
- Improve the on-road link across Power Street between Lennox Street and Grace Park
- Create a new underpass under the Railway Line between Lynch Street and Grace Park
- Investigate the feasibility of creating a shared path link along the length of the railway line through to Glenferrie Road

Figure 34 Example of residential development along a railway corridor



Public Realm

Landscape

- Landscape is to be placed in the front of buildings that are set back
- Existing mature trees are to be retained
- Heritage trees are to be protected
- Landscaping should not generally impede views of highly graded buildings
- Significant landscape is to be placed in public car parks
- All streets are to be landscaped with avenues of trees - regular planting of significant high branching canopy trees - to soften the impacts of the built form and parking
- Public art is to be strategically located in the public realm

Amenity

- Development is to occur that creates a safe pedestrian environment through design and increased surveillance – both real and perceived. See Safer Design Guidelines for Victoria, 2005
- Pedestrian paths are to be kept clear to ensure view corridors are retained and real and perceived safety enhanced
- Streets are to be well lit to enhance the public safety and contribute to the night time activity of West Hawthorn
- Loading bays, site storage and waste collection to be appropriately screened

Infrastructure

- Infrastructure capacity to cater for the needs of current and future users
- An on site detention policy applicable to the redevelopment of sites within the study area restricting stormwater discharges to a specified outflow
- Infrastructure design, location and materials are to contribute to the character and amenity of all precincts

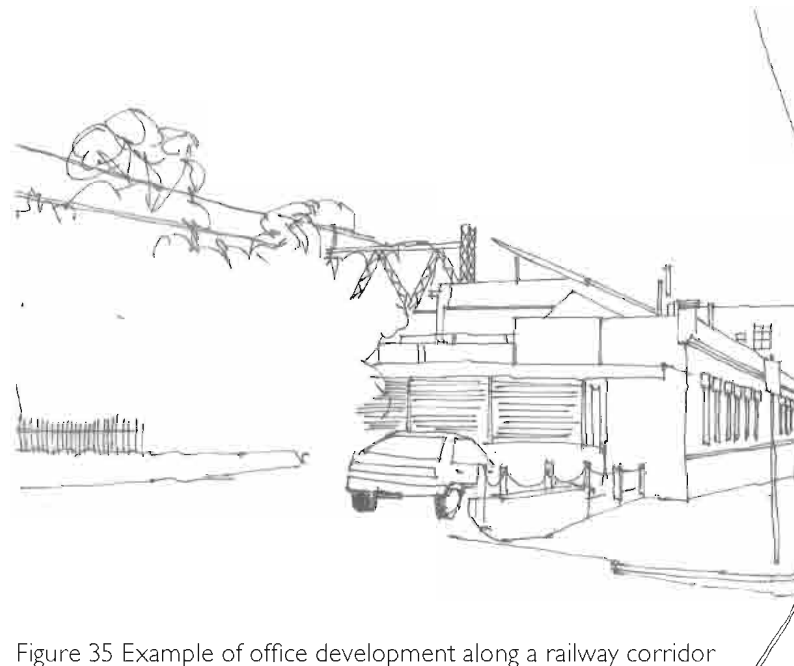


Figure 35 Example of office development along a railway corridor

ESD

- Water use, reuse and on site management employed
- Microclimate of open spaces to be managed by positioning:
 - Deciduous trees to the north to provide shade
 - Evergreen Vines to provide insulation
 - Use of vegetation to mitigate extremes of wind and direct required cooling breezes
- Use permeable surface coverings
- New development to provide bicycle facilities in accordance with Clause 52.34 in addition medium density residential development of more than three dwellings and office developments greater than 200sqm must provide parking of the same rates for other dwelling and office uses listed in Clause 52.34
- Incorporate Water Sensitive Urban Design where appropriate to improve quality of storm water leaving area

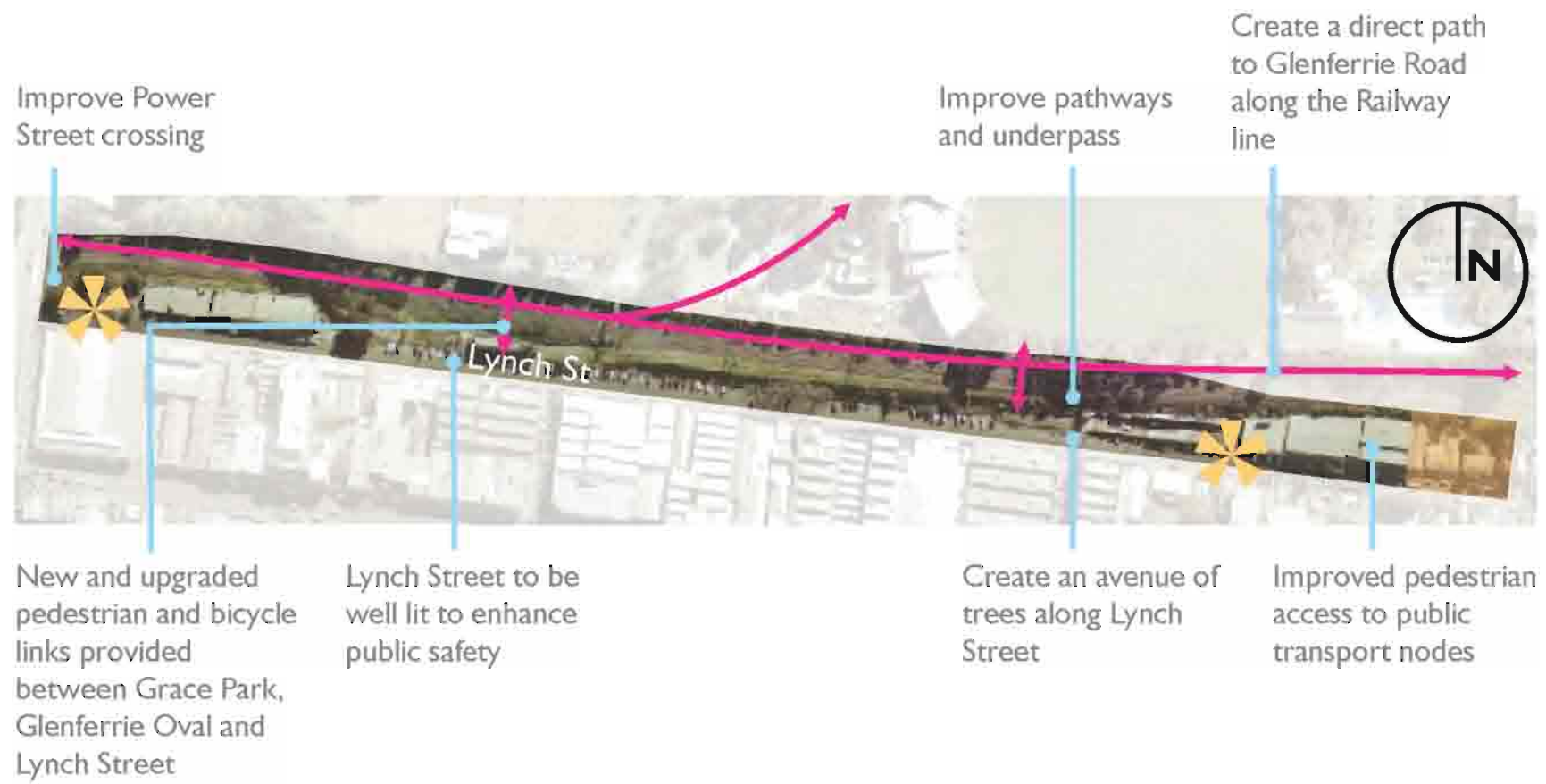


Figure 36 Precinct 5



5.7 Precinct 6 Glenferrie Road Quarter

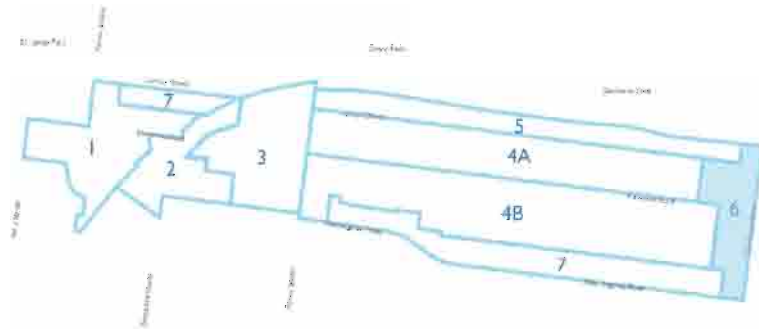


Figure 37 Precinct Plan showing Precinct 6

Key Objectives

- To promote appropriate land uses
- To promote new buildings to respect and compliment heritage buildings
- To ensure building scale reflects the character
- To promote a built form that does not compromise the amenity of adjacent residential land uses and minimises the negative impacts of:
 - overlooking
 - overshadowing
 - noise
 - visual bulk
- To promote landscape that does not compromise the amenity of adjacent residential land uses and minimises the negative impacts of:
 - overlooking
 - poor visual amenity
- To protect and enhance public realm amenity
- To ensure energy efficient design and practices are employed
- To ensure infrastructure capacity to cater for the needs of current and future users
- To ensure infrastructure design, location and materials are to contribute to the character and amenity of all precincts

Vision

This precinct has a strong retail character and has been classified as a Major Activity Centre. It will be subject to a future Structure Plan to be prepared by Council and the community. The concentration of heritage buildings in this precinct contributes to the precincts character. All new development or upgrades to existing buildings should occur in a complimentary manner and to be cognisant of residential interfaces.



Vehicular dominated environment in West Hawthorn



Example of high quality village character in Elwood

Preferred Outcome

Land Use

- Promote uses that reinforce existing retail and commercial uses including community and professional services

Built Form

- Medium built form
- Built form that does not compromise the amenity of adjacent residential land uses and minimises the impacts of overlooking, over shadowing, noise and visual bulk - refer to the standards contained within Clause 55 of the Planning Scheme and the City of Boroondara Residential Design Policy
- Well articulated building design that mitigates overlooking, overshadowing, noise and responds appropriately to the public realm. Refer to Guidelines for Higher Density Residential Development (DSE 2004) and Activity Centre Design Guidelines (DSE 2005) and Clause 55 of the Planning Scheme
- Exteriors or facades of buildings of recognized heritage value to be retained
- New buildings to complement existing heritage buildings
- In heritage areas carports and garages should be located to the rear of existing buildings
- Replacement of more than two traditional narrow buildings with a single wider building is to be discouraged
- New development to extend across the lot at ground floor to prevent concealed spaces and to continue consistent character on Burwood Road
- Facades and side walls that can be viewed from adjacent dwellings or roads are to be articulated
- Car parking and garage doors are to not dominate the streetscape
- Active frontages are to be developed along Burwood Road and Glenferrie Road
- Active frontages are to be developed, where it does not compromise the amenity of adjacent residential land uses with respect to noise, loss of privacy and poor visual amenity, along Lutton Lane
- Basement car parks should be fully located below street level when fronting Burwood Road and Glenferrie Road

Building Height and Front Setback

- Development above existing shop fronts is to be set back so that it cannot be seen above the apparent street wall height when viewed from the opposite side of the street at 1.7 metres above the ground (average standing eye level)



Figure 38 Example of a front setback for shoptop development that could be applied on Glenferrie Road

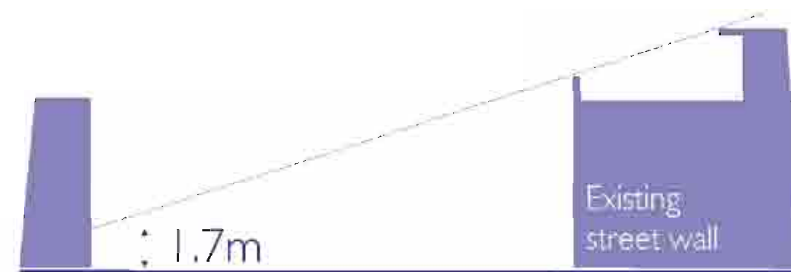


Figure 39 Example of a front setback for shoptop development that could be applied on Glenferrie Road

See Figure 40

- Buildings to be a maximum of 5 storeys in height.
- At 0.0 meters setback (the front boundary line) buildings are to be no greater than 10.0 meters in height. This results in a width to height ratio of 1:0.5 at the front boundary line
- Buildings are to be of a height and setback such that they maintain a street width to building height ratio of 1:0.5 at the front boundary line. Subsequent built form above 10.0 meters - behind the front boundary line - is to be stepped back so that it is out of sight when viewed from a height of 1.7 meters at the front boundary line on the opposite side of the street

5 Storey maximum (Ratio 1:0.5)

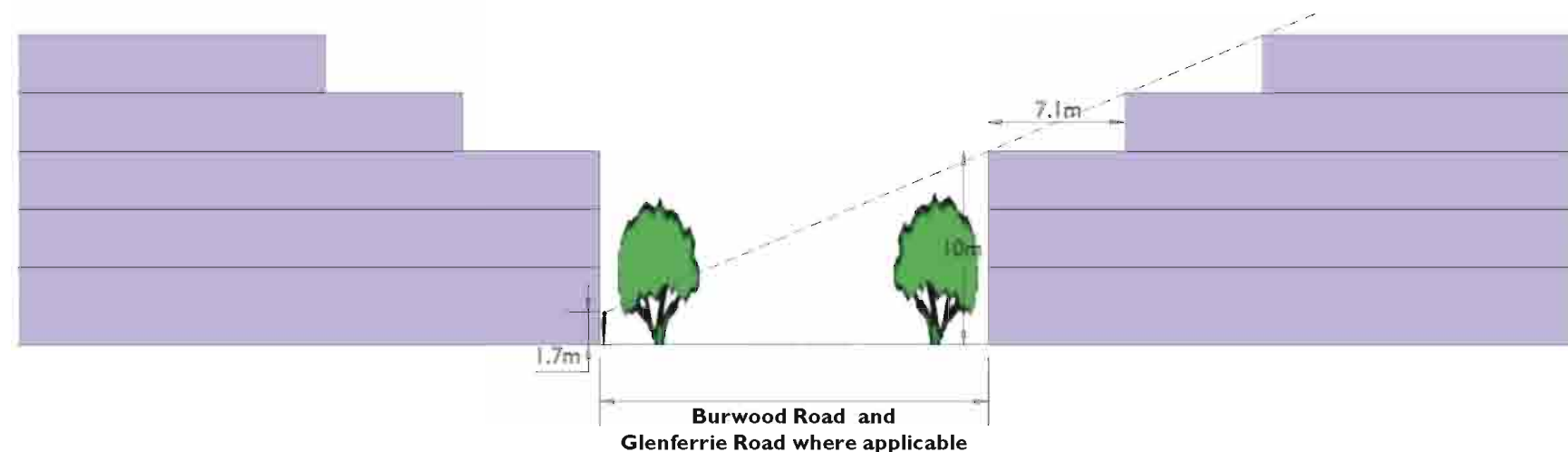


Figure 40 Front setback along Burwood Road

Side Setback

- Development on Burwood Road is to extend across the full width of the lot at least at ground floor level except where access requirements do not allow this

Car Parking

- All long term staff and resident parking generated by development sites to be accommodated onsite and measures/incentives will be provided to encourage users to use other transport modes (eg. walk, cycle and public transport)
- General standardisation of time restrictions throughout the precinct, subject to the retention of flexibility to tailor restriction times to cater for the parking activity generated by specific uses to provide consistency, and easier compliance/enforcement
- Time limit period for on-street parking to be from 8:00am to 6:00pm Monday to Saturday in commercial precincts and 8:00am to 6:00pm Monday to Friday in residential areas
- Parking management used to assist protecting residential streets from the intrusion of long-term employee parking
- Parking concessions granted for well designed extensions/refurbishments of existing heritage buildings
- Car parking for new development to be provided in accordance with Clause 22.03 and Clause 55 of the Boroondara Planning Scheme
- New developments that prepare 'integrated transport plans' to actively encourage the use of sustainable transport modes in the precinct be eligible for reduced on-site parking provision

Movement

- Develop a cycling and walking strategy for West Hawthorn
- Develop improved directional signage in the public realm for West Hawthorn
- Large employers within the project area to be encouraged to develop Green Travel Plans
- Introduce minor extension of 'No Stopping' zones on the approach/ departure of major intersections to increase intersection capacity

- Improved information particularly at tram stops, preferably with real time transport information
- Promote and respond to programs that encourage greater use of public transport such as
 - Linking Melbourne: Metropolitan Transport Plan
 - Growing Victoria Together
 - The Sustainable State (TSS)
 - Victorian Greenhouse Strategy
- Instituting a public relations campaign to sell the benefits of public transport, including reducing road congestion, noise pollution and greenhouse gas production and to raise the status of and provide a feel good factor for those using public transport
- Explore the possibility of introducing bus services through the precinct, to fill gaps in the existing public transport network (e.g. Burwood Road to Camberwell junction)
- Extend tram route along Burwood Road from Power Street to Camberwell Junction subject to feasibility study
- Directing funds obtained through parking revenues towards sustainable transport initiatives
- Improve signage to increase the general awareness of the available facilities
- Ensure pedestrian pathways comply with the DDA 1992
- Improve safety and accessibility of the existing pathways and linkages through lighting, landscaping treatments, routine vegetation maintenance, widening, linemarking, and compliance with DDA provisions

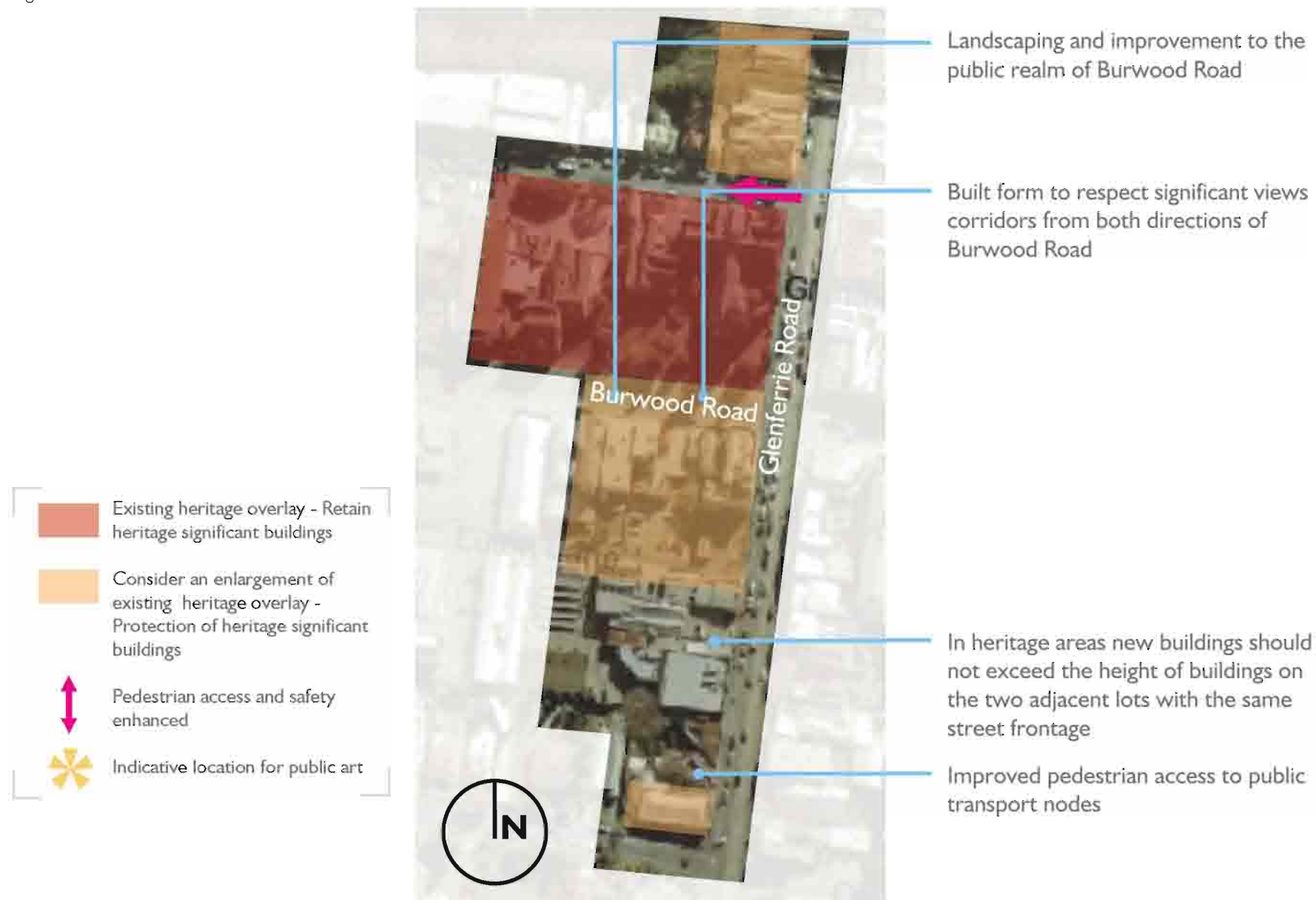
Public Realm

Landscaping

- Landscaping is to be placed in the front of buildings that are set back
- Existing mature trees are to be retained
- Heritage trees are to be protected
- Landscaping should not generally impede views of highly graded buildings

- All streets are to be landscaped with avenues of trees - regular planting of significant high branching canopy trees - to soften the impacts of the built form and parking
- Public art is to be strategically located in the public realm
- Landscaping and improvement to the public realm on Burwood Road
- Encourage landscape in front of buildings that are setback
- Heritage trees to be protected
- Landscaping to not impede views of heritage buildings

Figure 41 Precinct 6



Amenity

- Development is to occur that creates a safe pedestrian environment through design and increased surveillance – both real and perceived. See Safer Design Guidelines for Victoria, 2005
- Pedestrian paths are to be kept clear to ensure view corridors are retained and real and perceived safety enhanced
- Streets are to be well lit to enhance the public safety and contribute to the night time activity of West Hawthorn
- Loading bays, site storage and waste collection to be appropriately screened

ESD

- Water use, reuse and on site management employed
- Microclimate of open spaces to be managed by positioning:-
 - Deciduous trees to the north to provide shade
 - Evergreen Vines to provide insulation
 - Use of vegetation to mitigate extremes of wind and direct required cooling breezes
- Use permeable surface coverings
- New development to provide bicycle facilities in accordance with Clause 52.34 in addition medium density residential development of more than three dwellings and office developments greater than 200sqm must provide parking of the same rates for other dwelling and office uses listed in Clause 52.34
- Incorporate Water Sensitive Urban Design where appropriate to improve quality of storm water leaving area

Infrastructure

- Infrastructure capacity to cater for the needs of current and future users
- An on site detention policy applicable to the redevelopment of sites within the study area restricting stormwater discharges to a specified outflow
- Infrastructure design, location and materials are to contribute to the character and amenity of all precincts

5.8 Precinct 7 Established Housing

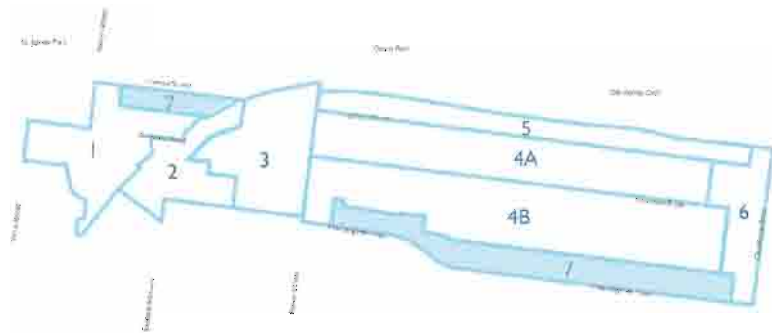


Figure 42 Precinct Plan showing Precinct 7

Key Objectives

- To retain and protect valued residential character
- To promote a built form that does not compromise the amenity of adjacent residential land uses and minimises the negative impacts of:
 - overlooking
 - overshadowing
 - noise
 - visual bulk
- To promote landscape that does not compromise the amenity of adjacent residential land uses and minimises the negative impacts of:
 - overlooking
 - poor visual amenity
- To protect and enhance public realm amenity
- To ensure energy efficient design and practices are employed
- To ensure infrastructure capacity to cater for the needs of current and future users
- To ensure infrastructure design, location and materials are to contribute to the character and amenity of all precincts

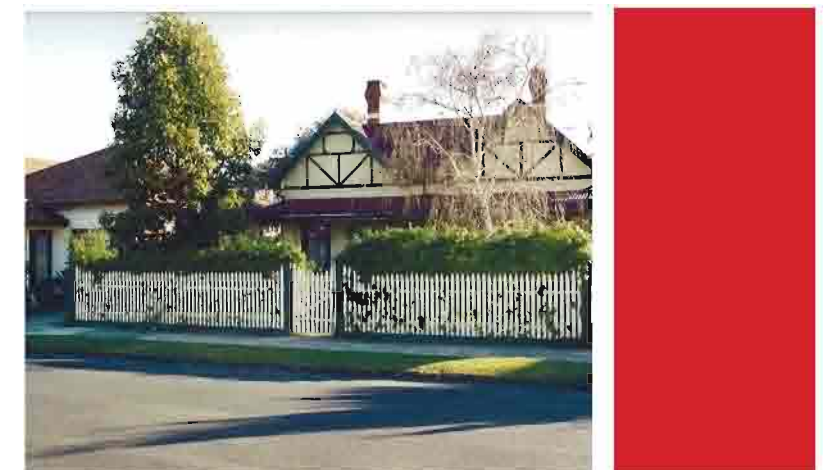
Vision

The vision for this precinct is to retain and protect the existing residential character and where upgrading existing built form is to occur, it must be sensibly managed.

Highly valued residential amenity in West Hawthorn



Figure 43 Rear interface March Equinox 3pm



Heritage significant residential character in West Hawthorn

Preferred Outcome

Land Use

- Retain and protect residential land use

Built Form

- Low built form
- Set backs to be in accordance with Res Code and the City of Boroondara Residential Design Policy.
- Built form that does not compromise the amenity of adjacent residential land uses and minimises the impacts of over- looking, over shadowing, noise and visual bulk - refer to the standards contained within Clause 55 of the Planning Scheme and the City of Boroondara Residential Design Policy.
- Exteriors or facades of buildings of recognized heritage value to be retained
- In heritage areas new buildings should not exceed the height of buildings on the two adjacent lots with the same street frontage
- New buildings to complement existing heritage buildings

Car Parking

- All long term resident parking generated by development sites to be accommodated onsite and measures/ incentives will be provided to encourage users to use other transport modes (eg. walk, cycle and public transport).
- General standardisation of time restrictions throughout the precinct, subject to the retention of flexibility to tailor restriction times to cater for the parking activity generated by specific uses (eg Capers and the Elgin Inn) to provide consistency, and easier compliance/ enforcement.
- Time limit period for on-street parking to be 8:00am to 6:00pm Monday to Friday.
- Maintain adequate parking for local residents
- Residential developments to meet the resident and visitor parking rates of Clause 55.03 – 11 of the Boroondara Planning Scheme

Movement

- Develop a cycling and walking strategy for West Hawthorn
- Develop improved directional signage in the public realm for West Hawthorn

ESD

- Water use, reuse and on site management employed
- Microclimate of open spaces to be managed by positioning:-
 - Deciduous trees to the north to provide shade
 - Evergreen Vines to provide insulation
 - Use of vegetation to mitigate extremes of wind and direct required cooling breezes
- Use permeable surface coverings
- Bicycle parking and end of trip facilities to be included in all new development
- Incorporate Water Sensitive Urban Design where appropriate to improve quality of storm water leaving area
- Incorporate Water Sensitive Urban Design where appropriate to improve quality of storm water leaving area

Infrastructure

- Infrastructure capacity to cater for the needs of current and future users
- An on site detention policy applicable to the redevelopment of sites within the study area restricting stormwater discharges to a specified outflow
- Infrastructure design, location and materials are to contribute to the character and amenity of all precincts



Highly valued residential amenity in West Hawthorn



Valued residential character with high quality landscape environment in West Hawthorn

Figure 44 Precinct 7



-  Existing heritage overlay - Retain heritage significant buildings
-  Consider an enlargement of existing heritage overlay - Protection of heritage significant buildings
-  Pedestrian access and safety enhanced
-  Indicative location for public art

6.0 West Hawthorn Improvements Strategy

Figure 45 Overall Strategy/ Concept



- ① Utilise changes in the public realm, landscape etc, to mark the beginning of the centre and slows traffic
- ② Built form to respect significant view corridors from both directions on Burwood Road
- ③ Improve bicycle parking facilities at Hawthorn Station
- ④ Improved pedestrian access to all public transport nodes
- ⑤ In heritage areas carports and garages should be located to the rear of existing buildings
- ⑥ Provide tram stop shelter at the existing tram stop
- ⑦ Shops abutting the Hawthorn Village carpark to create an active frontage
- ⑧ Provide tram seating
- ⑨ Retain local shopping village frontage
- ⑩ Widen the access point to Hawthorn Square front carpark
- ⑪ Improve accessibility across Power Street
- ⑫ Enhance the existing pathway to create a direct path to Glenferrie Road along the Railway line
- ⑬ Discourage residential land uses at ground floor level facing Burwood Road
- ⑭ Lynch Street to provide access to staff car parking area / back of house / service areas and for short term customer goods pickup
- ⑮ Improved pedestrian safety and accessibility across Burwood Road by installing pedestrian crossing facility
- ⑯ Landscape and improvements to the public realm on Burwood Road.
- ⑰ Cafes, bars and restaurants located at ground level along Burwood Road to service employment and residential land uses in and around the study area
- ⑱ Short term paid parking along Lynch Street
- ⑲ Extension of the tram route along Burwood Road from Power Street to Glenferrie Road
- ⑳ Lynch Street to be well lit to and provide high quality landscape to enhance public safety and amenity
- ㉑ Improved pedestrian access to public transport nodes
- ㉒ Potential bus link along Burwood Road turning right into Power Street
- ㉓ In heritage areas new buildings should not exceed the height of buildings on the two adjacent lots with the same street frontage
- ㉔ Parking management and enforcement to limit all day non-resident parking
- ㉕ Establish an area in front of West Hawthorn Shops as an important local space for passive activity
- ㉖ Residential development that does not undermine the economic viability of the dominant land uses
- ㉗ In heritage areas carports and garages should be located to the rear of the existing building

Pedestrian Links	Extended tram route	Existing heritage overlay - Retain heritage significant buildings
Enhance existing pathway	Pedestrian crossing	Consider an enlargement of existing heritage overlay - Protection of heritage significant buildings
Possible bus link		

7.0 Glossary of Terms

Active Frontages: Refers to street frontages where there is an active visual engagement between those buildings in the street and those on the ground floors of buildings. This quality is assisted where the front façade of buildings, including the main entrance, faces and is open towards the street. (Source: Activity Centre Design Guidelines, Department of Sustainability and Environment)

Amenity: A quality used to describe an environment that evokes pleasantness, comfort and convenience.

Built Form: The 3-dimensional characteristics of buildings and the built environment.

Clause 22.03: Car Parking Policy (Source: Boroondara Planning Scheme)

Clause 55: Two or more dwellings on a lot and residential buildings (Source: Boroondara Planning Scheme)

Ecologically Sustainable Development (ESD): Ecological Sustainable Development is development that meets the needs of the present without compromising the ability of future generations to meet their own needs. (Source: The Brundtland Commission 1987)

Egress: A path or opening for going out; an exit. (Source: WWW Dictionary)

Equinox: Either of the two times during a year when the sun crosses the celestial equator and when the length of day and night are approximately equal typically on or around March 21 and September 21. (Source: WWW Dictionary) The equinox is used in planning policy for determining appropriate levels of sunlight to spaces.

Ingress: A means or place of entering. (Source: WWW Dictionary)

Interface: A term used to describe the relationship/ interaction between different land uses.

Key Objectives: Refers to overarching goals that if applied will achieve the vision for the study area. The objectives set the framework for the Preferred Outcomes.

Legibility: A term used to describe how easily people can understand the layout (links and movement network of a place).

Rescode: Is a package of provisions for residential development that came into effect across Victoria on 24 August 2001. Rescode is not a single document - the Rescode provisions are incorporate into the Planning Schemes and the Building Regulations. Rescode refer Clause 55 of the Boroondara Planning Scheme.

Principles: The Principles express and outline a vision for the study area. The key principles in the WHUDF were established after undergoing an extensive period of consultation and collaboration with the community and Council.

Public Realm: Space between buildings that is in public ownership, consisting of all outdoor areas, including public squares, roads and access ways, surface car parking areas as well as the portions of buildings open to the public.

Setback: Distance that a building or a part of building is set back from a property boundary or a significant part of the building.

Solstice: Either of two times of the year when the sun is at its greatest distance from the equator. (Source: WWW Dictionary) The summer solstice in the Southern Hemisphere occurs about December 21 when the Sun is furthest south and the length of time between Sunrise and Sunset is the longest of the year. The winter solstice occurs about June 21 when the Sun is towards north and the length of time between Sunrise and Sunset is the shortest of the year.

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