



Maling Road Built Form Design Guidelines

Adopted by Council 17 March 2014 (updated 11 June 2015)



BOROONDARA
City of Harmony

DOCUMENT STRUCTURE

This document consists of two (2) parts:

- Part 1: Built Form Guidelines and Implementation
- Part 2: Background Information.

Part 1: Built Form Guidelines and Implementation contains the built form objectives, guidelines and strategies for the Centre and details of how these guidelines will be implemented through the Boroondara Planning Scheme. The contents of this section will be used as the basis for the statutory planning controls that will be introduced into the planning scheme through an amendment to the Boroondara Planning Scheme.

Part 2: Background Information contains details of the document review (which includes a review of submissions to the Draft ACS), and built form/urban design analysis that were carried out to inform preparation of the built form controls detailed in Part 1. The document is intended to act as a supplementary document to Part 1 to provide the reader with additional information on the basis for the objectives, guidelines and strategies detailed in Part 1.

Adopted by Council on: 17 March 2014

Revision no: 1 **Date:** 20 February 2014

Revision no: 2 **Date:** 29 May 2014 - Minor correction to errors in text

Revision no: 3 **Date:** 25 June 2015 - Updated to reflect Council's resolution from 11 June 2015

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Introduction

BACKGROUND

The Maling Road Shopping Village (the Centre) is a neighbourhood shopping Centre located along Maling Road between Canterbury Road and Scott Street, Canterbury.

The Centre contains around 70 businesses that serve a convenience retail function for local residents (with basic items provided by a small supermarket, fresh food outlets, a newsagency/post office, chemist, bank and hairdressers) as well as a specialist destination Centre in areas such as fashion and homewares. The Centre enjoys a metropolitan wide reputation for its village atmosphere, specialist antiques shops, arts and crafts, fashion boutiques and eateries.

The Centre forms part of Heritage Overlay HO145 - Maling Road Shopping Centre and Residential Environs and contains a number of significant heritage buildings that enhance the character of the Centre. One property in the Centre is on the Victorian Heritage Register; Canterbury Mansions (formally known as Malone's Family Hotel) at 208 Canterbury Road, Canterbury (HO29). The Canterbury Post Office at 206 Canterbury Road, Canterbury is on the Commonwealth Heritage Register.

The Centre was identified in the *Draft Boroondara Activity Centres Strategy 2011* (Draft ACS) as a Neighbourhood Activity Centre - Level 3. Level 3 Centres was the classification given to lower order shopping Centres that provided a day-to-day convenience role to residents.

The Draft ACS was placed on public consultation between November 2010 and March 2011. As a result of the consultation process Council received more than 400 submissions from residents in relation to Maling Road. As a result of strong

community opposition, Council resolved to remove the Centre from the Draft ACS when it adopted the strategy on 6 June 2011.

As a result of the Centre being removed from the Draft ACS, now known as the Neighbourhood Shopping Centres and Commercial Corridor Guidelines, Maling Road is the only neighbourhood shopping Centre in the municipality that does not have strategic direction in the Boroondara Planning Scheme regarding its future built form.

PROJECT SCOPE

The overarching objectives for the Maling Road project were to:

- Review the built form controls and respond to submissions to the Draft ACS;
- Develop revised built form guidelines for future development within the Centre (including building heights and building setbacks); and
- Develop and implement statutory planning controls to guide future built form outcomes within the Centre.

The project scope includes the following deliverables:

- Review of submissions to the Draft ACS and other relevant planning policies.
- Built form/urban design analysis to develop clear built form objectives, guidelines and strategies relating to:
 - Building heights;
 - Building setbacks; and
 - Any other key design elements.

- Preparation of statutory planning controls to implement the built form guidelines into the Boroondara Planning Scheme; and
- Processing a Planning Scheme Amendment, including public consultation.

The project scope dealt solely with built form and does not include a review of land uses in the Centre.

Part 2 of this document summarises the review of submissions and built form analysis undertaken. This background work was used to inform the built form controls detailed in Part 1 of this document.

STUDY AREA

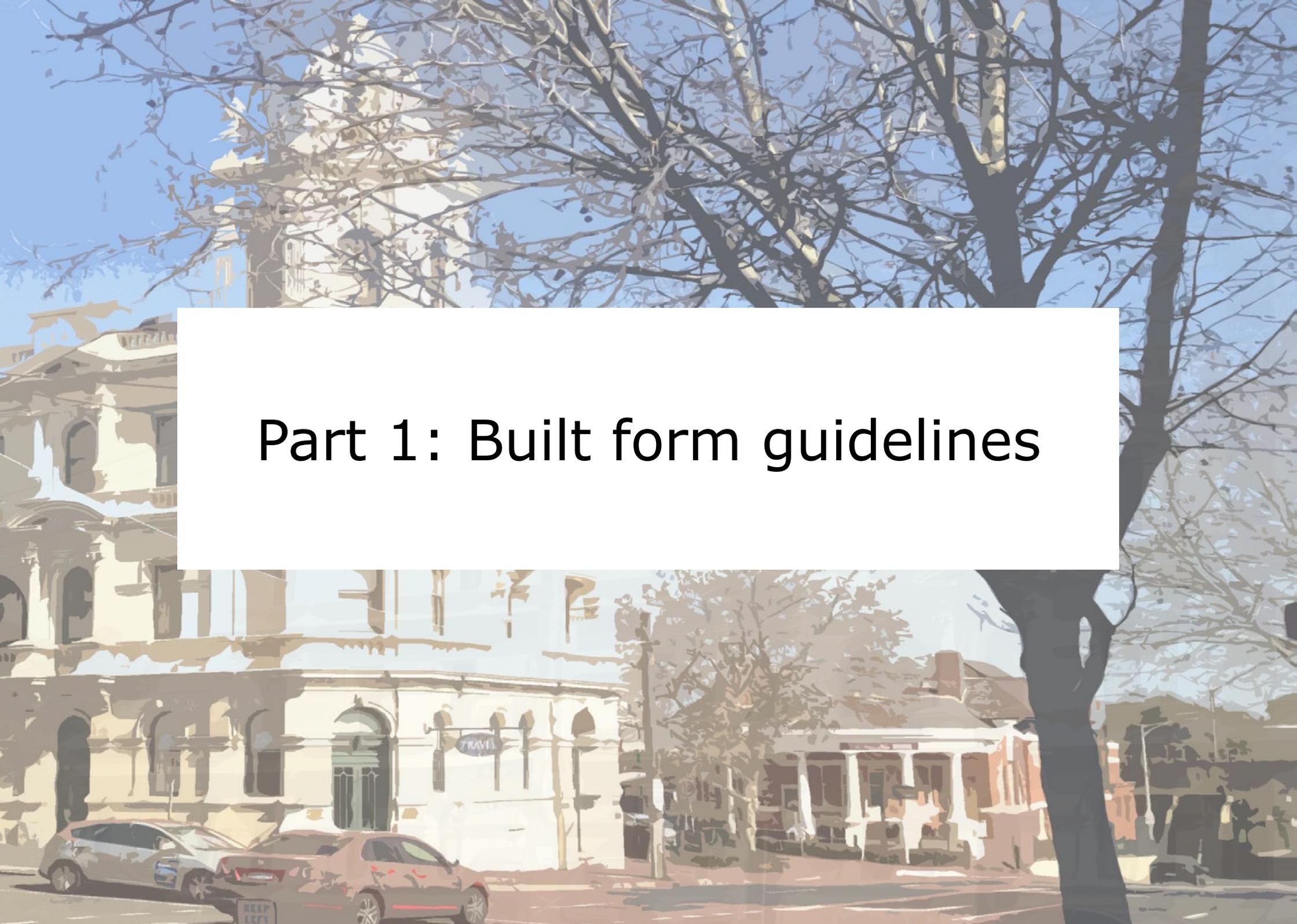
The study area is defined in Map 1. This map also shows the revised boundary for the Maling Road Shopping Centre and the area to which the built form guidelines in the document apply.

The boundary of the centre has been refined from the boundary proposed by the Draft ACS. The changes to the boundary remove Council owned car parks and properties at 210 to 216 Canterbury Road and 2B Wattle Valley Road. The boundary was extended to include 85 Maling Road. However, Council resolved to remove 85 Maling Road from the Centre's boundary on 20 October 2014.

The properties at 210 to 216 Canterbury Road and 2B Wattle Valley Road are to be included in the Canterbury Road Commercial Corridor Area 6 and included in Design and Development Overlay Schedule 20 (DDO20). The Properties were removed from the Maling Road Precinct as they have a stronger relationship with the Canterbury Road Commercial Corridor.



Map 1: Study area

A street scene featuring a large, ornate, light-colored building with arched windows and a prominent entrance. The building is partially obscured by the bare branches of a large tree in the foreground. To the right, there is a smaller, single-story building with a covered porch. Several cars are parked or driving on the street. The sky is clear and blue.

Part 1: Built form guidelines

Vision and Objectives

VISION

The vision statement expresses the key elements of the Centre that Council believes should guide development. The vision for Maling Road is:

'Maling Road Neighbourhood Shopping Centre will build on its location and architectural value to enhance its reputation and village atmosphere. The character of the Centre will be maintained and enhanced by providing development opportunities that respect the heritage value of the area.'

OBJECTIVES

To assist in achieving the above vision, the following overarching objectives have been developed:

- To preserve and maintain the historic streetscape of Maling Road.
- To retain short and long range vistas to Canterbury Mansion.
- To maintain the varied building heights and low rise scale that is characteristic of the Maling Road.
- To ensure new buildings and additions respect and enrich the historic streetscape of Maling Road.
- To respect the character and amenity of surrounding residential areas.
- To improve the safety and amenity of public spaces including pedestrian walkways, station access, Theatre Place and public car parks.

Future development should only be supported if it achieves these objectives.



Photo 1: Significant heritage buildings in the Maling Road streetscape



Photo 2: North site of Maling Road, looking south from the corner of Byrson Street

Design Guidelines

Any buildings and works in the Centre should meet the following design guidelines.

1. BUILDING HEIGHTS AND STREET SETBACKS

The key characteristic of building heights and setbacks in Maling Road are:

- Its mix of 1 and 2 storey buildings that give the streetscape a low rise varied appearance. Historic buildings in the Centre range from approximately 5 metres to 9 -10 metres to the top of the parapet. Ceiling heights are lower than the parapet height; approx 8 metres (based on 3.6 metre floor to ceiling height).
- Hard built edge to Maling Road.

The low rise scale of the Centre also allows short and long ranges vistas to the Canterbury Mansion on the southeast corner of the intersection of Maling Road and Canterbury Road (landmark building shown on Map 2). In particular its tower is a prominent landmark within the Centre and can be seen from the surrounding area.

The scale of buildings in the Centre also contributes to the Centres 'village feel' and intimate pedestrian environment.

Design guidelines

Design guidelines to retain vistas and the low rise character of the Centre are:

- Buildings heights in the Centre should be restricted to ensure view lines to landmark buildings are retained and the

low rise character of the Centre is protected by:

- Applying a height limit of 11 metres to existing 2 storey 'significant' and 'contributory' heritage buildings.
- Applying a height limit of 8.5 metres to existing single storey 'significant' and 'contributory' heritage buildings.
- Applying a maximum height limit of 10.5 metres to 60-78 Maling Road.
- Applying a maximum height limit of 8.5 metres 6 Bryson Street (non-contributory property) to retain the low rise character of this section of the Maling Road streetscape.

- New development of commercial properties must be built to the street edge (zero setback) at ground level.
- Upper level additions to 'significant' and 'contributory' heritage places shown on Map 2 should be set back a minimum of 5 metres behind the street wall. A greater setback may be required for upper level additions to single storey 'significant' or 'contributory' heritage buildings to protect heritage values.
- The street wall of new developments on 'non-contributory' properties in Map 2 should reflect the street wall height of any abutting 'significant' or 'contributory' heritage buildings. If there are no abutting heritage buildings the street wall should not exceed 8.5 metres in height and read as no more than 2 storeys from the street. Development over a height of 8.5 metres should be setback a minimum of 5 metres behind the street wall.

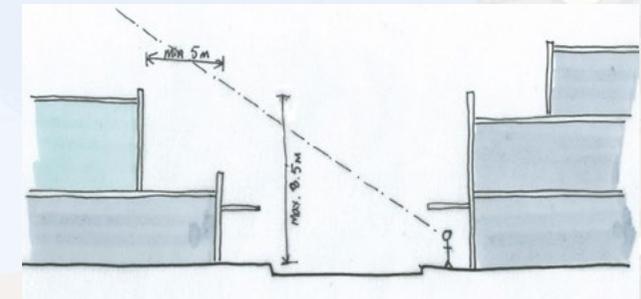


Figure 1: Building height and upper level setback for single storey 'contributory' and 'significant' heritage buildings

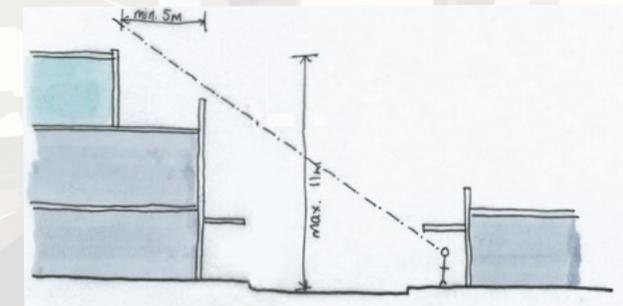
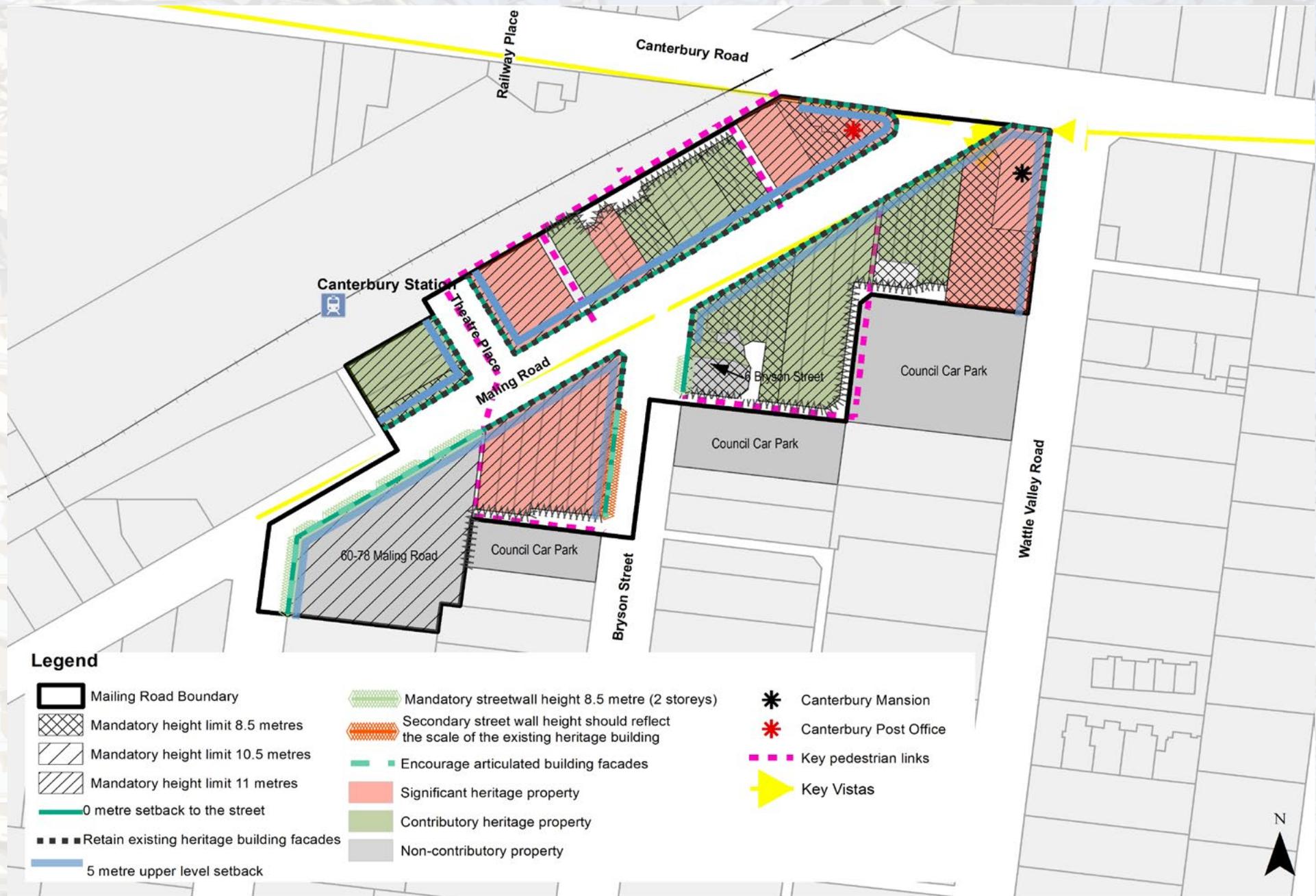


Figure 2: Building height and upper level setback for double storey 'contributory' and 'significant' heritage buildings.

Note: 6 Bryson Street has a maximum height limit of 8.5 metres. As such no upper level setbacks apply to this property.



Map 2: Building heights and street setbacks

2. BUILDING FACADES

The historic buildings that make up the Maling Road streetscape are integral to the character and village feel of Maling Road. Loss of original building fabric will have a detrimental effect on the streetscape of Maling Road and erode the building fabric that is fundamental to the character that the community values.

The key built form features in Maling Road that contribute to the streetscape are:

- The high degree of intactness of the early 1900's Maling Road streetscape which has experienced minimal changes.
- Regular subdivision pattern which typically results in lot frontages of 5 metres to 8 metres.
- Vertical architectural detailing and proportions of buildings and architectural elements such as windows.
- Consistent use of external materials including rendered or face brick finish.
- Feature architectural elements such as feature tiles to shop fronts, pressed metal ceilings and detailing.

Design guidelines

Clause 22.05 Heritage Policy provides extensive design guidance for the facades of properties that are in Heritage Overlays. Including policy direction to:

- Retain existing 'significant' and 'contributory buildings.
- Ensure upper level additions are recessive elements in the streetscape.
- Ensure new buildings and works use building materials that complement heritage buildings.
- Ensure buildings and works do not make significant

changes to the building facade that could diminish the heritage value of the property or precinct.

In additions to the policy direction in Clause 22.05 the following Design guidelines should be followed:

- The visible elements of "significant" and "contributory" buildings that make up the streetscape of Maling Road, should be retained and restored.

Design guidelines for 60-75 Maling Road are provided in Section 4.



Photo 3: 108-112 Maling Road



Photo 4: 114 -122 Maling Road



Photo 5: 103-109 Maling Road; Upper level addition to 109 Maling Road



Photo 6: 94 Maling Road, 2 and 6* Bryson Street

*Existing non-contributory properties to the Maling Road heritage precinct.

3. INTERFACE WITH PUBLIC SPACES

Public car parks, pedestrian walkways, laneways

The majority of properties in Maling Road have two frontages; a well-defined edge to Maling Road and a secondary laneway frontage.

The laneways are currently underutilised but offer key routes for pedestrian access to the station and public car parks. The laneways provide an opportunity to improve safety and amenity in these public spaces, in particular pedestrian access to Canterbury Railway Station and the public car parks on Wattle Valley Road and Bryson Street. These areas could become unique laneways that make a positive contribution to the village character of Maling Road.

Current laneway characteristics include:

- Undefined built edge to the laneway.
- Poor natural surveillance of laneways and pedestrian routes.
- Used primarily for services i.e waste, storage, on-site parking etc.

Design guidelines

Properties that have a secondary frontage to a public car park, laneway or pedestrian walkway should, where practical:

- Incorporate pedestrian access, outdoor eating and/or shop fronts and windows at ground level.
- Orientate windows and balconies on upper levels to the laneways.
- Screen service areas from public view.
- Incorporate lighting into the design of buildings and provide clearly visible entrances.

Theatre Place

Theatre Place is a public plaza located in the centre of the shopping centre. The plaza acts as one of the main pedestrian links to Canterbury Station and is the only public space in the centre that provides a place for people to sit and gather.

Development of the building on the north side of the plaza (119 Maling Road) has the potential to further overshadow this public space, which already experiences extensive overshadowing during the day. This building is a double storey building with an overall height of approximately 9.5 -10 metres.

Ensuring this space is not further overshadowed by development is important to maintain amenity in this public space.

Design guidelines

To protect the amenity of the public space the following guideline should be met:

- Development of 119 Maling Road should not exceed a height of 11 metres.
- The height and setback requirements applied to street frontage must be applied to properties fronting Theatre Place.



Photo 7: Theatre Place mid morning



Map 3: Interfaces with public spaces and site specific controls

4. SITE SPECIFIC DESIGN GUIDELINES

60 - 78 Maling Road

The Post Office and Canterbury Mansion building at the northern end of the Maling Road shopping strip act as landmark buildings which identify the start of Maling Road from Canterbury Road.

At the southern end of the Centre No. 60-78 Maling Road are the gateway properties to the Maling Road shopping strip. The properties have characteristics that are atypical of the properties in the rest of the Centre and do not reflect the historic character of the remaining buildings along Maling Road. These sites also have the most redevelopment potential.

Future redevelopment of this property is important given its location at the southern entrance to the centre. The redevelopment of this property presents an opportunity to improve the southern entry into the Centre.

The site also directly abuts residential properties outside of the boundaries of the Centre. Careful and sensitive treatment of this interface is important to the amenity of the abutting residential properties

Design guidelines - building facades

To ensure redevelopment enhance the Maling Road streetscape the following design guidelines should be followed:

- The facade of new developments should:
 - Be no more than two storeys at the street frontage.
 - Demonstrate high quality architectural design that complements the heritage properties in the Centre.
 - Provide frequent pedestrian access and glazing at street level to activate the street and facilitate interaction between the internal ground floor uses and

the public realm.

- Incorporate vertical and horizontal architectural elements that reinforce the rhythm and fine grain character of the historic Maling Road streetscape.
- Avoid blank, unarticulated walls along primary and secondary street frontages.
- Incorporate weather protection above all footpaths. Awning heights should be continuous and match the awning heights of abutting significant or “contributory” heritage building.
- Provide setbacks from the street which have regard to the setbacks of abutting heritage buildings.

Design guidelines - sensitive residential interface

To protect the amenity of abutting residential properties and provide an appropriate entry into the Centre, the following guidelines should be met:

- Provide an appropriate transition along the sensitive residential interface shown on Map 3 by ensuring development adjacent to residential properties is no more than one storey higher than the abutting “significant” or “contributory” heritage building by:
 - Setting back the side or rear boundary a minimum of 3 metres up to 8 metres. Over a height of 8 metres the side or rear setback should be in accordance with Clause 55.04-1 Standard B17 of the Boroondara Planning Scheme. (Refer to Figure 3).
- A side or rear elevation that abuts a ‘sensitive residential interface’ shown on Map 3 should:
 - Be articulated to reduce visual bulk and its dominance when viewed from Maling Road and Scott Street. Sheer, blank, unarticulated walls will not be supported.

- Ensure the length of any side elevation responds to the siting and length of side walls of historic residences in the surrounding area.
- Ensure the third floor is a recessive element. Use of lightweight materials and finishes are encouraged for development over a height of 8 metres.
- New development should be designed to minimise amenity impacts on adjoining residences and demonstrate how overlooking and overshadowing impacts have been addressed in accordance with the objectives and standards of Clause 55.04 Standards B21 and B22 of the Boroondara Planning Scheme. The setback should be landscaped with plants that will provide visual screening of the building from surrounding residential areas.

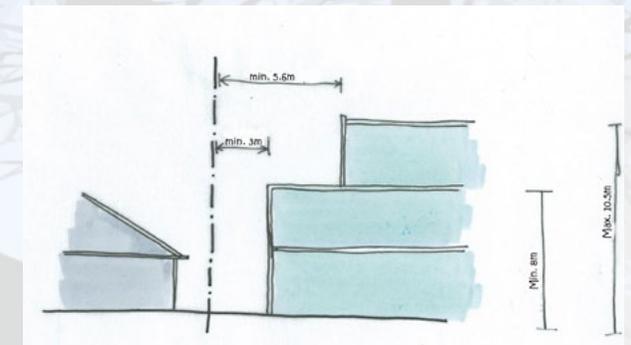


Figure 3: Setbacks from sensitive residential interfaces shown on Map 3 and 4.



Legend

- | | | | |
|---|---|---|------------------------|
| Maling Road Boundary | Mandatory streetwall height 8.5 metre (2 storeys) | Sensitive residential interface setback applies | Canterbury Mansion |
| Mandatory height limit 8.5 metres | Secondary street wall height should reflect the scale of the existing heritage building | Encourage active frontages and natural surveillance | Canterbury Post Office |
| Mandatory height limit 10.5 metres | Encourage articulated building facades | Elevations to be articulated to reduce visual bulk | Key pedestrian links |
| Mandatory height limit 11 metres | Significant heritage property | | |
| 0 metre setback to the street | Contributory heritage property | | |
| Retain existing heritage building facades | Non-contributory property | | |
| 5 metre upper level setback | | | |

Map 4: Maling Road built form design guidelines

Implementation

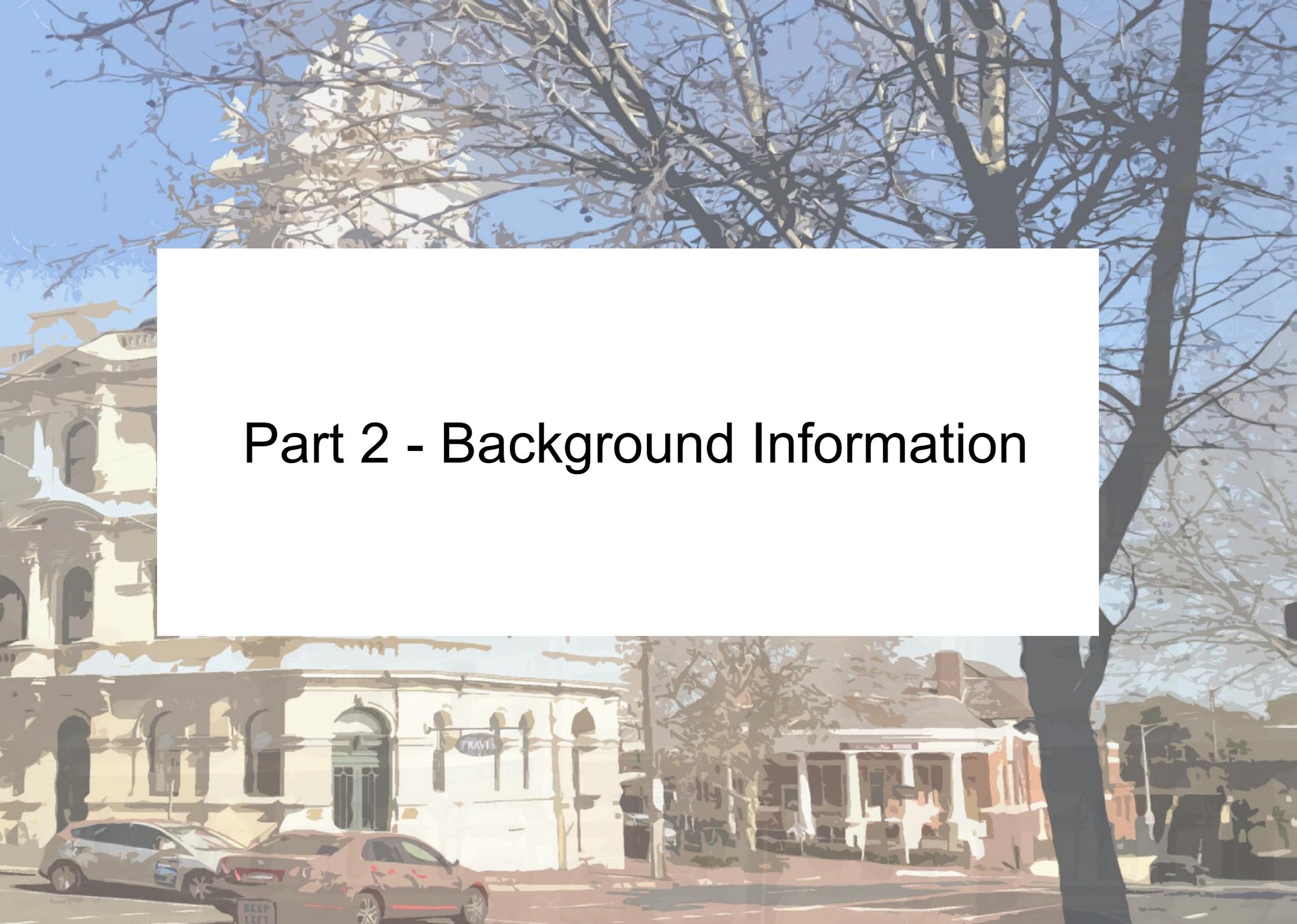
STATUTORY IMPLEMENTATION

Statutory implementation for the study area will be achieved by preparing and processing a planning scheme amendment which:

- Applies Design and Development Overlay Schedule 23 (DDO23) to the Maling Road Neighbourhood Shopping Centre as defined in Map 4.
- Applies Design and Development Overlay Schedule 16 (DDO16) Canterbury Road Commercial corridor to 210 to 216 Canterbury Road and 2B Wattle Valley Road and includes the properties in Area 3a which allows for development up to 11 metres with a 3 metre landscaped setback to Canterbury Road. Note: Amendment C108 replaced interim DDO20 with DDO16 on 9 April 2015. When the MRBFG were prepared and initially adopted by Council DDO20 was in the Boroondara Planning Scheme.
- Applies the Environmental Audit Overlay (EAO) to 85 Maling Road to reflect past use of the site as a garage to trigger an environmental audit prior to the use of the site for sensitive land uses, such as residential uses.

FURTHER WORK

- Upon approval and gazettal of Amendment C108 to the Boroondara Planning Scheme, investigate whether or not the draft Clause 22.12 Urban Design Policy should apply to Maling Road and whether draft Clause 22.08 Activity Centre, Neighbourhood Shopping Centres and Commercial Corridors needs to be updated to include Maling Road.
- Investigate changes to the draft Clause 22.07 Neighbourhood Character Policy once introduced into the planning scheme (forms part of Amendment C190) to include DDO23 in the list of areas this policy does not apply to.
- Continue to work with the Environment and Sustainable Living Department on the Shopping Centres Improvement Programme which provides an ongoing program of streetscape improvements to shopping Centres in the municipality, subject to funding. This includes public realm improvements to the Maling Road shopping Centre.
- Investigate the feasibility of working with Victrack to make various improvements to access to the Canterbury Railway Station and making better use of the shared pedestrian spaces, railway station car park and under croft of the railway line.

A street scene featuring a white building with arched windows and a red car parked on the street. A large tree with bare branches is in the foreground, and a blue sky is visible in the background. A white rectangular box is overlaid on the image, containing the text "Part 2 - Background Information".

Part 2 - Background Information

Document Review

1. DRAFT BOROONDARA ACTIVITY CENTRE STRATEGY 2011 AND SUBMISSIONS

The Draft Boroondara Activity Centre Strategy 2011 (Draft ACS) identified Maling Road as a Level 3 Neighbourhood Shopping Centre.

The Draft ACS vision for Maling was:

'Maling Road Neighbourhood Activity Centre will build on its location and architectural value to enhance its reputation and village atmosphere. The character of the Centre will be maintained and enhanced by providing development opportunities that respect the heritage value of the area.'

This vision is still considered to be relevant for this Centre.

The preferred role for the shopping centre in the Draft ACS was:

'...The Centre will not undergo significant change. A small amount of additional development will occur. This will comprise discreet additional levels to buildings along Maling Road, with some development on land behind the shops for mixed use or residential development. The Centre will strengthen its metropolitan-wide reputation of a village atmosphere.'

The Draft ACS propose a height limit of 12 metres for all buildings in the Centre. It also envisaged a street wall height of 8 metres for the centre, with development over 8 metres setback a minimum of 5 metres behind the street wall.

Review of the submissions received during the consultation period for the Draft ACS in 2011 identified the following key concerns:

- Neighbourhood character: In particular preservation of existing heritage character and opposition to proposed building heights (although there was misconception that building heights of 4-6 storeys were being proposed)
- Concern that Council car parks were going to be redeveloped.
- Traffic and car parking impacts from increased development.

The community strongly objected to the designation of the Centre as a Neighbourhood Activity Centre and felt that the built form controls did not adequately protect the unique character of Maling Road.

Officers have reviewed the submissions to inform the revised design guidelines for this centre. Officers responses to the issues raised by residents to the Draft ACS are summarised below:

Neighbourhood Character and heritage

Maling Road is covered by a Heritage Overlay (HO145) which triggers a planning permit for any alterations and additions to properties in this centre.

The overarching objective of the Heritage Overlay is to conserve and enhance heritage places and those elements which contribute to the significance of heritage places.

The presence of the Heritage Overlay over Maling Road gives Council the ability to consider the impact new development may have on the heritage fabric of Maling Road. This overlay and Council's local Heritage Policy at Clause 22.05 of the planning scheme inherently control aspects of built form such as building heights, upper level setbacks and alterations and additions to heritage facades.

The Draft ACS sought to apply additional built form controls to the centre by applying a blanket height limit of 12 metres for the entire centre. It also proposed a maximum street wall height of 8 metres with development over 8 metres being setback a minimum of 5 metres.

As a result of the built form analysis undertaken, some of the built form controls proposed in the draft ACS have been revised to better protect the Maling Road Streetscape and provide a more site specific design response. These changes are detailed in Part 1 of this document.

Traffic and car parking

Provision of parking in the centre is controlled by Clause 52.06 Car Parking and Parking Overlay Schedule 1. These clauses provide parking rates for land uses in the centre and guidelines for when parking dispensations could be considered by Council.

The introduction of built form controls will not change the current process for council considering parking rates or dispen-

sation for parking as part of planning permit applications.

With respect to traffic, this is an ongoing concern of residents and an issue that is typically dealt with at planning permit application stage. In the case of Maling Road, the growth potential of the centre is constrained by the Heritage Overlay that affects all properties in the centre, and the attributes of properties in the centre. It is unlikely that this centre will see significant growth and any growth or additional floor space is unlikely to significantly exacerbate any existing traffic conditions.

For these reasons traffic and car parking have not been considered in the scope of the built form review.

Redevelopment of Council Car Parks

When the Draft ACS was exhibited there was a public perception that the inclusion of Council owned car parks meant that these parcels of land were going to be redeveloped. However, that was not the case and Council has no plans to redevelop these sites.

To alleviate these concerns, Council resolved to remove Council owned car parks from the ACS. Consistent with this resolution the Council owned car parks at 1A and 8 Bryson Street and 3 Wattle Valley Road are to be removed from the Maling Road shopping centre boundary. Accordingly, these sites are not proposed to be included in the proposed Design and Development Overlay.

2. CURRENT PLANNING POLICY

The following statutory planning controls apply to Maling Road at the time of preparing this document:

- Commercial 1 Zone
- Clause 43.01 Heritage Overlay Schedule 145 (HO145) and HO29 (208 Canterbury Road - listed on the Victorian Heritage Register)
- Clause 45.09 Parking Overlay Schedule 1
- Clause 22.05 Heritage Policy
- Clause 52.06 Car Parking

Heritage, parking and traffic

As discussed, issues relating to heritage, parking and traffic for this centre are already dealt with in the Boroondara Planning Scheme. The built form controls proposed for Maling Road do not need to replicate the controls that are already in place, rather provide additional guidance over and above the controls that already exist and ensure there are no contradictions or duplication between the proposed built form controls for Maling Road and those that already apply to the centre.

SWOT Analysis - Built Environment

The purpose of the strengths, weaknesses, opportunities and threats (SWOT) analysis was to analyse what attributes of the centre give it its character and what attribute or development that might have a negative impact on Maling Road.

The design objectives and guidelines developed have been tailored to reinforce the key strengths and manage the weakness and opportunities so that they have a positive influence on the Centre.

The key strengths, weaknesses, opportunities and threats for the centre are summarised below and in Map 5.

STRENGTHS

The Centre's key strengths include:

- Intact historic streetscape with minimal changes.
- Village like feel created by its compact footprint and sense of street enclosure.
- The Canterbury Mansion (Malone Hotel) is an iconic building which is visible from the wider area and assist with way finding in the centre and for the wider area.

OPPORTUNITIES

The Centre's key opportunities include:

- Improve the southern gateway of the centre to create strong gateways at both entrances to the centre.
- Make better use of secondary frontage to council car parks and laneways to improve passive surveillance of these areas and encourage additional retail frontage in the centre.
- Improve passive surveillance of key pedestrian links to the station and car parks.
- Improve access to the stations and Canterbury Gardens through a joint venture with Victrack.
- Control the height of development to protect vistas to key landmark buildings.
- Control the height of development to protect the historic streetscape of Maling Road.
- Control upper level setbacks to ensure protection of the Maling Road streetscape.

WEAKNESSES

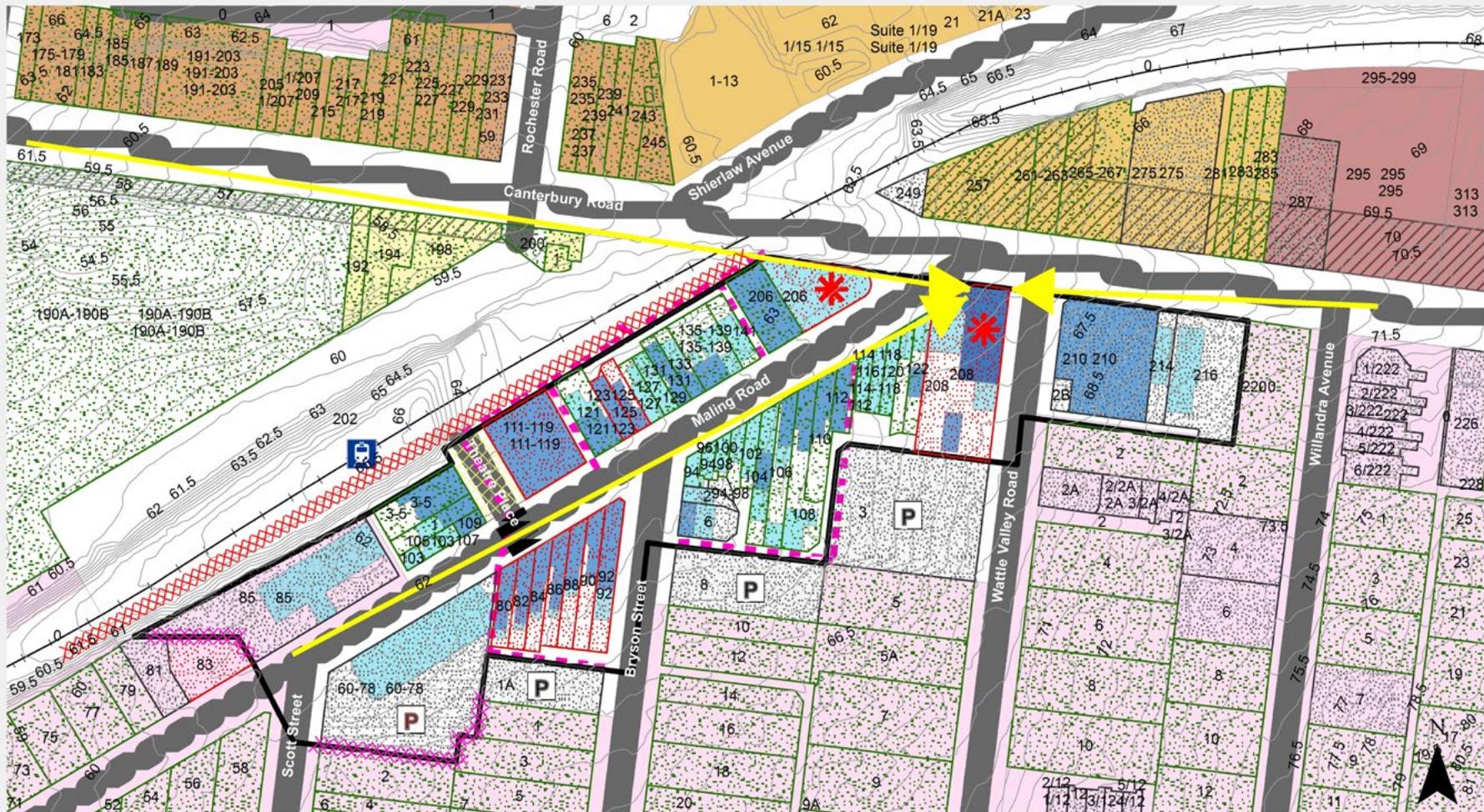
The Centre's key weaknesses include:

- Interfaces with/lack of passive surveillance of the key pedestrian pathways that link Maling Road shops with the railway station.
- Interfaces with/lack of passive surveillance of public car parks.
- Limited opportunity for further growth without impacting on the heritage streetscape of Maling Road.
- Potential impact of redevelopment of the sites at the southern gateway into the centres. Particular the potential visual bulk of wester/southern elevations of these buildings.
- Limited connection between 210-216 Canterbury Road and Maling Road.

THREATS

The Centre's key threats include:

- Re-development of non-contributory properties in the centre.
- Inappropriately sited development that impact the heritage significance of the 'significant' and 'contributory' buildings and the Maling Road streetscape.
- Reduced amenity in Theatre Place.
- Impacts on the amenity of surrounding residential areas.
- Constraining development potential in the centre to a point where it does not allow for natural renewal and regeneration of the centre and starts to impact on the centres economic viability.
- Visually bulky and dominate side elevations on 60-75 and 85 Maling Road which detract from the character of Maling Road and surrounds



Legend

Study Area	Key Pedestrian Link	Significant heritage place	Existing building heights	Proposed C108 building heights
Railway Line interface	Pedestrian Crossing	Contributory heritage place	1 Storey (approx 5 m)	Max. height 16 m (8m streetwall)
Residential Interface	Key Public spaces	Non-Contributory property	2 Storeys (approx. 9 m -10 m)	Max. height 16 m (8m streetwall)
Key Vistas	Landmark Buildings	Neighbourhood Residential Zone 3 (NRZ3)	3+ Storeys (over 9 m)	Max. height 11 m (9 m streetwall)
	Contours 50cm			Max. height 8 m (8m streetwall)

Map 5: SWOT Analysis

Built Form Analysis

The built form analysis considered:

- Building setbacks and heights
- Architectural features
- Building materials
- Lot widths and depths
- Access
- Public and sensitive interfaces.

A summary of the key attributes is provided below and summarised in Map 7.

SETBACKS

- Buildings built to the primary street edge creating a hard edge to Maling Road.
- Setbacks to laneways vary and are not consistent; no defined street edge.

BUILDING HEIGHTS

- No prevailing building height in the centre. Building heights vary; equal portions of 1 and 2 storey historic properties within no set pattern.
- Street wall heights of heritage buildings in the centre range from approx. 5 metres to 9 -10 metres to the top of the parapet.
- The Canterbury Mansion (Malone Hotel) building at the corner of Canterbury Road and Maling Road is the tallest building. The property is on the Victorian Heritage Register (VHR) and is a landmark building in the centre with a 3 - 4 storey scale.

BUILT FORM

- Original buildings in the centre range from the late 1800's to the early 1900's.
- Buildings were typically built as groups; typically pairs or groups of 5+ shops.
- Buildings typically have high street walls with parapets that conceals the roof form behind.
- The key horizontal built form element in the centre is the awnings above the footpath. It is a strong feature which visually links buildings in the centre.
- Buildings typically have a vertical emphases which is emphasized by narrow lot frontages (typically 5 metres to 8 metres), window proportions and vertical architectural detailing, particularly at property boundaries.
- Maling Road has a road reserve width of approximately 15 metres. This creates a strong sense of enclosure emphasized by the groups of 2 storey shops in the centre.
- Use of parapet detailing is strong throughout the centre.



Photo 8: View from the corner of Bryson Street and Maling Road, looking south.

BUILDING MATERIALS

- Historic buildings in the centre typically have a rendered or face brick finish and have feature architectural elements such as:
 - Feature tiles to shop fronts
 - Pressed metal ceilings and detailing
 - Use of a mixture of render and face feature brick work
 - Variation in brick size, finish and detailing
 - Brass and copper window frames.

LOT FRONTAGES

- The historic properties along Maling Road typically have narrow lot frontage of 5 metres to 8 metres. Some wider lots are scattered through the centre. This give the centre a fine grain built form.
- Lot depths vary; on the north side (railway side) lot depth are typically 28 metres. On the south side they range from approx. 13 metres up to 57 metres (Malone's Hotel). The narrow lot frontages and lot depths place limitations on the level of development that is feasible on properties in this centre and limits the ability for parking to be provided on-site.

STATION AND ACCESS

- Pedestrian access to the station from the Maling is poor and lacks natural surveillance.
- Natural surveillance and access to the Council car parks is poor and could be significantly improved.



Photo 9: View looking south along the laneway which provide pedestrian access to Canterbury Station.

PUBLIC AND SENSITIVE INTERFACES

- Secondary frontages to laneways, and public parking areas are not well defined and offers little natural surveillance of public spaces.
- 60-78 Maling Road shares a boundary with a properties in residential zones. These properties are located to the south and consideration needs to be given to potential amenity impacts of these properties. In particular, overshadowing, overlooking and visual bulk.
- Further overshadowing of Theatre Place will impact on the amenity of Theatre place.



Photo 10: View looking at the rear of shops that have a secondary frontage to the Bryson Street car park.

FUTURE DEVELOPMENT OPPORTUNITIES

- The rear of some significant and contributory heritage buildings offer some potential for additional development without impacts on the streetscape of Maling Road.
- 60-78 Maling Road at the southern end of the Centre are the sites most likely to undertake more substantial change in the future.
- Victrack landholdings around Canterbury Station could be better utilise and used to improve pedestrian access and amenity to the station.